

Channel Islands Harbor Public Areas Plan and Design Guidelines

Adopted June 24, 2008
County of Ventura Board of Supervisors



prepared for **County of Ventura Harbor Department**

prepared by **Wallace Roberts & Todd, Inc.
Selbert Perkins Design Collaborative
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Aerial photograph of Channel Islands Harbor

**RESOLUTION OF THE BOARD OF SUPERVISORS
OF THE COUNTY OF VENTURA
ADOPTING THE CHANNEL ISLANDS HARBOR
PUBLIC AREAS PLAN AND DESIGN GUIDELINES**

WHEREAS, the County of Ventura contracted with Wallace Roberts & Todd, Inc., to prepare a Public Areas Plan and Design Guidelines for use in redevelopment and reconstruction of Channel Islands Harbor; and

WHEREAS, the Harbor Department, with Wallace Roberts & Todd, Inc., held two public workshops, two Harbor Commission workshops, and two workshops at the Board of Supervisors to obtain the public's input on the public amenities in the Harbor; and

WHEREAS, notices of these public hearings were sent to interested individual and agencies, were included in the utility bills mailed to customers of the Channel Islands Beach Services District, mailed to City of Oxnard neighborhood council chairs, and mailed to slip tenants within the Harbor, residents of the Seabridge, Mandalay Bay, and Westport communities in the City of Oxnard; and

WHEREAS, Wallace Roberts & Todd, Inc., documented the findings from these public workshops, as well as their assessment of the existing conditions in Channel Islands Harbor in a report entitled "Channel Islands Harbor Opportunities and Constraints Report Landside Enhancements," dated April 24, 2008; and

WHEREAS, the DRAFT Channel Islands Harbor Public Areas Plan and Design Guidelines and the Opportunities and Constraints Report were released for public review on May 10, 2008, and have been available in paper format from the Harbor offices, on CD from the Harbor offices, and on the Harbor's website since that date; and

WHEREAS, the Channel Islands Harbor Public Areas Plan and Design Guidelines is intended to assist in the redevelopment of the Harbor parcels by encouraging cohesive public amenities such as parks, pedestrian promenades, landscaping, lighting, signage and parking areas; and

WHEREAS, the Channel Islands Harbor lessees have agreed to be bound by these Plans and Guidelines when designing their development projects; and

WHEREAS, the Channel Islands Harbor Public Areas Plan and Design Guidelines will be used to prepare the Landside Public Works Plan Amendment for the Channel Islands Harbor (PWPA #4), and in preparing any Notice of Impending Development documents for future development in the Harbor; and

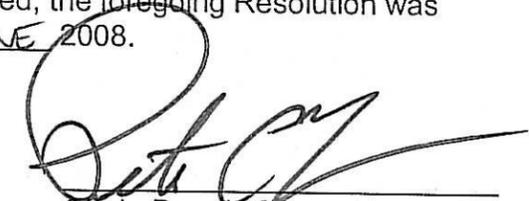
WHEREAS, the Channel Islands Harbor Public Areas Plan and Design Guidelines will assist the County in obtaining any grant funds or other assistance from other public agencies when developing park areas, or constructing other public facilities; and

WHEREAS, the Plan anticipates replacement and reconstruction of existing facilities, the addition of small park areas, signage and lighting, and improvements to parking areas and landscaping that are designed to improve the quality of storm water runoff into the Harbor and reduce the use of water for landscaping purposes.

NOW, THEREFORE, BE IT RESOLVED, ORDERED, AND DETERMINED THAT the Board determines that the approval of the Channel Islands Harbor Public Areas Plan and Design Guidelines is Categorically Exempt from the California Environmental Quality Act (CEQA) under Sections 15301, Existing Facilities, 15302, Replacement or Reconstruction, 15303, New Construction or Conversion of Small Structures, 15304, Minor Alterations to Land, and 15307 and 15308, Actions to Maintain, Protect, or Restore Natural Resources.

FINALLY, BE IT RESOLVED, ORDERED, AND DETERMINED THAT the Channel Islands Harbor Opportunities and Constraints Report and Channel Islands Harbor Public Areas Plan and Design Guidelines are hereby adopted.

Upon motion of Supervisor FLYNN, seconded by Supervisor LONG, duly carried, the foregoing Resolution was passed and adopted this 24th day of JUNE, 2008.


Chair, Board of Supervisors

ATTEST: Marty Robinson,
Clerk of the Board of Supervisors,
County of Ventura, State of California.

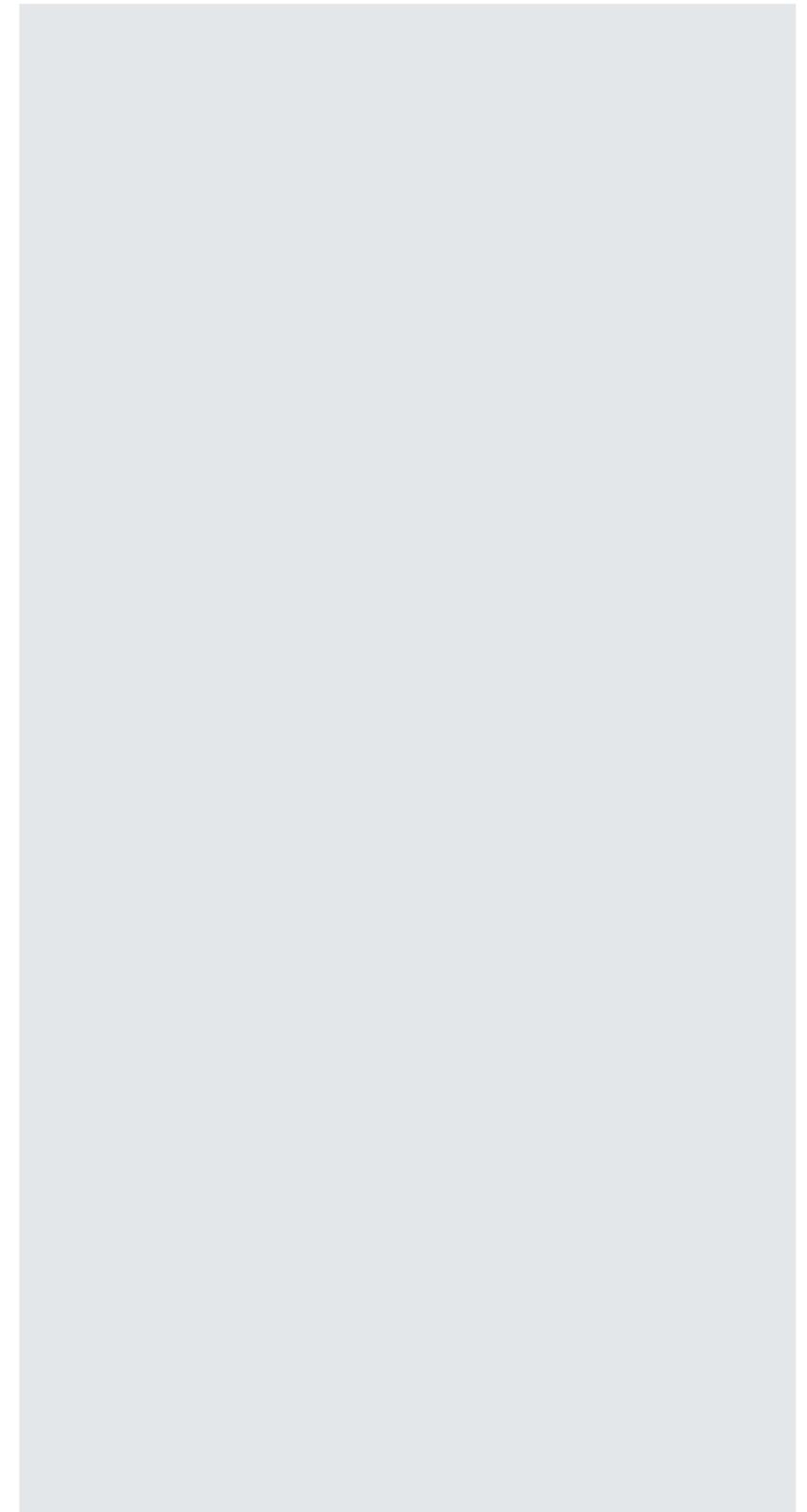
By: 
Clerk of the Board





contents

4	Reason for the Plan	56	Recommendations: Landscape
6	Goals for Channel Islands Harbor	74	Recommendations: Signage
8	How we got Here: Process, Assessment, Framework	98	Recommendations: Lighting
14	Recommendations: Public Walkways	120	Recommendations: Drainage & Water Quality
24	Recommendations: Parks	124	Recommendations: Pavement, Parking & Vehicular Circulation
28	Anacapa Park	130	Implementation
32	Santa Cruz Park	135	Appendix: Existing Conditions
38	Santa Barbara Park	144	Appendix: References
42	Santa Rosa Park	148	Appendix: Record of Final Public Hearing & Adoption
46	San Miguel Park		
54	Pocket Parks		



Improve public access & amenities

Provide cohesive design guidelines

Focus on walkways, parks, landscape, signage, lighting, water quality, parking & vehicular circulation

Channel Islands Harbor is a gem on the California coast between Port Hueneme and Oxnard. It was built as a recreational harbor in the 1960's and 1970's on about 310 acres of land, with approximately 2,200 boat slips, as well as marina facilities, restaurants, sportfishing facilities, chandleries, and shops. The Harbor's attraction to boaters, residents, and visitors also comes from its public parks, the pedestrian promenade that provides a pleasant walking experience, the low-key nature of development surrounding the Harbor, and its proximity to the Santa Barbara Channel and the Channel Islands National Park.

The Harbor is divided into three sections (west, east, and peninsula) served by separate public roads, with each section providing different services. The west side consists of marinas, a linear park, restaurants, residential development, and small retail businesses. The peninsula is dominated by hotel development, marinas, apartments, and condominiums. The east side is primarily commercial and serves boaters by offering boat yards, a marine supply store, boat sales, law enforcement, administration, and search and rescue facilities.



reason for the plan

In spite of its gem-like qualities, in some respects the Harbor still is a "diamond in the rough." Because its three sections lack both physical and visual unity, it is difficult to perceive as a single harbor. Development in the Harbor has not been guided by any overall design standards, resulting in a variety of architectural styles and a hodge-podge of street furniture, lighting, and signage. The promenade is not continuous, and the existing parks are not outfitted with ample opportunity for playing, picnicking, and enjoying the water. A proliferation of sign types clutters the Harbor landscape, and irregular lighting makes some areas of the Harbor too bright at night, while other areas are very dark and feel unsafe. In addition, some of the landscaped and paved areas around the Harbor suffer from lack of care.

The purpose of this plan is to improve the public areas of the Harbor so they are attractive to a wide range of residents and visitors, and to tie all three areas of the Harbor together as one common destination with an updated visual identity. The Plan offers uniform design standards that can be applied to future development and redevelopment projects, and proposes public amenities that enhance the experience for boaters, residents, and visitors. When implemented, the Harbor's enhanced look and safety will help visitors and residents enjoy the many different activities available in Channel Islands Harbor. These guidelines also are intended to gradually improve environmental quality, including stormwater discharged into the Harbor.



“To offer recreational and business opportunities to the residents of Ventura County, and tourism opportunities to the general public through the operation of the Channel Islands Harbor in a manner which meets our fiduciary responsibility to Ventura County and its residents.”

- Channel Islands Harbor mission statement

goals for channel islands harbor

- **Refine identity and image**
- **Improve pedestrian access and waterside promenade**
- **Safety, accessibility, comfort**
- **Variety of uses**
- **Visitor-friendly**
- **Reduce stormwater pollution**
- **Native and regional landscaping**
- **Easy maintenance and affordability**

- Refine the Harbor's image and identity through cohesive signage, lighting, and landscape improvements that are attractive and up-to-date.
- Improve pedestrian access to the water's edge, and create a continuous pedestrian promenade around the Harbor, to the maximum extent possible.
- Improve access to marinas from parking lots.
- Recommend landside improvements that enhance the Harbor experience while also maintaining high standards for safety, accessibility, and comfort.
- Plan public spaces that can accommodate a variety of uses.
- Improve signage and wayfinding elements to make the Harbor more visitor-friendly and market the Harbor's businesses and events.
- Reduce the amount of impervious surfaces to improve water quality, mitigate the heat island effect, and make paved areas more attractive.
- Improve stormwater runoff quality.
- Recommend landscape plantings that have minimal requirements for irrigation, chemical inputs, and other maintenance, and are regionally appropriate.
- Recommend improvements that are affordable to build and maintain.



Channel Islands Harbor: Overview of Existing Uses

Legend

- ③ Water Taxi Stop
- ⋯ Water Taxi Route
- ▭ Park
- ▭ Commercial/Business Area



Overview of Channel Islands Harbor area

how we got here: process

“Let’s encourage business that is successful. This is a beautiful place.”

“Shops thrive with public interest areas.”

“Organize your efforts and this area will revitalize.”

“One theme for the whole harbor!”

“Must create a Channel Islands National Park theme.”

“Most important! Complete a walkway around the harbor.”

“Better access along Victoria—right now it’s dangerous for pedestrians.”

The recommendations for public enhancements in Channel Islands Harbor have been informed by different stakeholders. Direct public responses were obtained in two workshops held at the Harbor. Feedback also was provided by the Channel Islands Harbor Commission in three separate meetings, and by the Ventura County Board of Supervisors at two study sessions. Additional comments came from a local walking group, Harbor lessees, and Harbor staff. All of the information has supplemented the assessment of Harbor landside features conducted by the project team (see also “Channel Islands Harbor: Opportunities and Constraints Report—Landside Enhancements,” February 2008).

Public Workshops

The first public workshop, held on August 22, 2007, was attended by approximately 175 people at the Lobster Trap Restaurant in Channel Islands Harbor. It had the following goals:

- Introduce the project scope and objectives.
- Describe the preliminary findings from assessment of the Harbor’s landside features, and present options for enhancing these elements.
- Obtain public comments on the assessment and possible enhancements.

This information was presented to the public at five stations that were staffed by project team members, who were available to answer questions and take comments. Workshop attendees expressed their comments and questions verbally (at the opening and closing of the workshop, and at the individual stations) and by filling out comment cards.

Three major themes were distilled from the public at this first workshop. First, any improvements should maintain the focus on water at the Harbor—in terms of recreation as well as physical and visual access. Second, the plan should respect the integrity of surrounding neighborhoods, where residents appreciate the area’s scenic beauty and quiet lifestyle. Third, the Harbor’s proximity to the Channel Islands provides an opportunity to re-imagine the Harbor’s identity and public space design, by emphasizing the connection to the Channel Islands’ unique natural history, as well as the area’s cultural history through educating Harbor visitors about Chumash heritage.

The second public workshop was held at the Embassy Suites near the Harbor on October 24, 2007. Approximately 100 people attended. The format was a formal presentation of preliminary concepts for some of the



Participants at first public workshop

Harbor’s public spaces, with several opportunities for facilitated verbal comment; written comments were collected as well. The majority of comments reflected the following concerns:

- Financing and maintenance of Harbor improvements
- The importance of having a continuous waterside promenade
- Pedestrian safety along Victoria Avenue
- Possible conflicts among pedestrians and bicyclists along sidewalks
- Security, vandalism, and graffiti
- Making the Harbor a destination
- Having equitable distribution of improvements among all three sections of the Harbor
- Preserving views to the water



Presentation during second public workshop

Harbor Commission Meetings

The Channel Islands Harbor Commission was provided with project updates on three separate occasions: September 19, 2007; November 28, 2007; and January 16, 2008. At these meetings, the Harbor Commission was given a brief summary of work to date, followed by preliminary concepts and recommendations for their consideration. These three meetings allowed Commissioners the opportunity for facilitated questions and comments.

Ventura County Board of Supervisors Meetings

Two study sessions convened by the Ventura County Board of Supervisors allowed each Board member to comment on preliminary recommendations for landside enhancements. These sessions were held on October 2, 2007 and February 12, 2008.

Other Input

A meeting with a group of Harbor walkers was held August 8, 2007, to hear more specific concerns related to the pedestrian experience in Channel Islands Harbor. Comments from Harbor lessees were provided on November 12, 2007, after a project update presented at a meeting of the Lessees’ Association. Other review and comment was made by a biologist that has been monitoring heron nesting activities in the Harbor, a City of Oxnard parks maintenance supervisor, and Channel Islands Harbor staff members.

“Less concrete and more green areas and benches.”

“Don’t obstruct Harbor view with signs, lights.”

“Prevent water pollution.”

“Must stand up to marine environment.”

“Design with consideration to vandalism.”

“Who will maintain it?”

“Timeless or maritime feeling—not trendy.”

“Let’s think big picture and long term!”

how we got here: assessment

- **Walkways should be accessible to persons with disabilities and boaters**
- **Add native landscaping**
- **Provide children's play areas**
- **Improve wayfinding signs**
- **Lighting should be consistent**
- **Pavement needs resurfacing**
- **Bioswales for stormwater collection**

The project team conducted an inventory and assessment of the Harbor's public walkways, parks, landscape, signage, lighting, drainage, and parking and vehicular circulation. A summary is presented in "Channel Islands Harbor: Opportunities and Constraints Report—Landside Enhancements." The assessment informed the objectives and criteria that underlie the recommendations presented in this report. It highlighted aspects of the Harbor's public realm that can benefit from comprehensive design guidelines, and revealed unique strengths of the Harbor on which the public improvements and design guidelines can be built. The following points briefly summarize the assessment, with improvement opportunities shown on page 11.

Walkways

- Discontinuous walkways need to be connected and completed
- A continuous walkway is needed along Victoria Avenue
- Increase public access to water edge
- Walkways should be accessible to persons with disabilities and boaters with dock carts
- Increase plantings along walkways
- Provide frequent areas to stop and enjoy Harbor views

Parks and Landscape

- Increase public park space & add more pocket parks
- Add more native, drought-tolerant, lower-maintenance plantings
- Increase areas of landscaping
- Screen undesirable views (e.g., utility and trash enclosures)
- Provide more site furniture with a consistent appearance
- More children's play areas are needed

Signage

- Need updated gateway signage to identify Harbor and highlight three main sections (west, peninsula, east)
- Current signage has too many messages; broad mix of tenant signage adds to confusion
- Need a consistent style that works with lighting and site furnishings
- Improve informational signage at vehicle entries and other nodes
- Improve wayfinding around the Harbor
- Opportunity for interpretive signage in public areas

Lighting

- Broad range of light levels—from excessively bright to no light at all
- Need more attractive and uniform security lighting along promenade and in parks and parking areas
- Minimize off-site light and glare; lighting should not interfere with boating and water navigation
- Enhance gateways and destinations
- Current mix of fixture styles; should be more consistent
- Reduce energy consumption and minimize maintenance costs

Drainage, Water Quality, Pavement, Parking and Vehicular Circulation

- Need to replace or resurface most pavement within five years, to improve drainage, and add landscaping and stormwater filters
- Some paved areas will need to be re-graded to improve drainage
- Bioswales and/or mechanical filters will be needed to comply with future stormwater regulations
- Need more efficient parking and vehicular circulation on west side

Channel Islands Harbor: Opportunities and Constraints

- Promenade
- Promenade-Street Connectors
- Existing Parks
- Park Expansions/New Parks
- Potential Pocket Parks
- Existing Restrooms
- Proposed Restrooms
- Existing Guest Docks
- Proposed Guest Docks
- Gateway and Entries
- Banners
- Improved Parking Areas



Channel Islands Harbor opportunities and constraints

how we got here: framework

- Proximity to Channel Islands
- Unique opportunity for interpretive exhibits
- Highlight significant connection to Chumash culture
- Sailing history
- Regional facility that serves Ventura County and beyond

The nearby Channel Islands have emerged as a key place-making component for the Harbor. Their visual presence is a constant in the lives of area residents, and their national significance is emphasized by the establishment of Channel Islands National Park and Marine Sanctuary. This plan recommends that the design framework for the Harbor's public realm integrate the Channel Islands, such that their unique natural and cultural histories inform public space design.

"Islands at the Harbor"

The Harbor's proximity to the Channel Islands provides a wonderful opportunity to re-imagine the Harbor's identity. Public improvements can be expressed through using native plants in the landscape (including those endemic to the Channel Islands), incorporating Channel Islands' plants and animals in design motifs and public art, and describing the Channel Islands and Chumash culture through interpretive exhibits in



public areas. Moreover, the relationship between the Channel Islands and sailing vessels provides another thread of material to integrate in design, and reinforces the Harbor's role as a boating facility. Finally, the Harbor's parks can be themed as different islands, which can guide park design, programming, and interpretive features.

An "Islands at the Harbor" framework does the following:

- Provides a unique identity for the Harbor and its businesses
- Inspires a range of design graphics and landscape plant palette
- Offers guidance for design of parks in the Harbor
- Allows Harbor visitors to learn more about the Channel Islands and surrounding areas

Other Considerations

In addition to a design framework based on the Channel Islands, other features of Channel Islands Harbor are important to the recommendations in this plan. These include its role as a County recreational facility, and its residential context.

Because it is a County facility that offers recreational boating access, Channel Islands Harbor serves people throughout Ventura County and beyond. This potentially broad user base can benefit from other Harbor amenities in addition to boating facilities. Improved public spaces can educate County residents and other visitors about the area's unique resources, and improvements that attract and retain visitors also support the vitality of the Harbor's landside facilities.

It is important to note that the Harbor is near residential areas immediately to the west, north, and south-southeast. Residents have chosen to have homes here in part because of the area's scenery, beach access, and peaceful lifestyle. Area residents also appreciate being able to walk or bike the Harbor's sidewalks, patronize its businesses, enjoy its occasional events and farmer's market, and use its boating facilities and parks. Although the Harbor is a County facility with a range of commercial, visitor and marine-related businesses, its proximity to residential areas suggests that any Harbor improvements respect the neighborhood context.



Putting it all Together

By considering the design framework and other features discussed above, the Harbor's public realm can maximize visual and physical access to the water, incorporate nodes for activity (play, events, gathering) and quiet reflection (intimate spaces, views), and express the natural and cultural richness of the Channel Islands.

Although the Plan components (parks, signage, lighting, etc.) are discussed separately in this report, they are intended to work together when implemented. For example, improved pedestrian access will not only bring people to the water's edge as much as possible, but it will also connect park spaces and Harbor businesses. Public access, circulation, and safety will be made easier by a comprehensive system of lighting and informational signage. Lighting, signage, and site furnishings also will contribute to Harbor identity and aesthetics through updated and uniform design. Additional landscaped areas along walkways and in parks and parking lots will provide more greenery with a plant palette that reflects the regional flora, frames important views, and helps improve water quality through coordination with drainage infrastructure. Harbor landscaping, combined with lighting and interpretive signage, also will evoke the area's unique themes: Chumash culture, maritime and nautical features, and the natural history of the Channel Islands.



The Harbor's different outdoor areas will:

- Allow a variety of experiences for Harbor visitors
- Increase the Harbor's ability to simultaneously accommodate different uses
- Enable "storytelling" in the Harbor's three main areas (west, peninsula, and east)
- Maximize view opportunities and access to Harbor businesses and amenities
- Present a cohesive and legible appearance to visitors that reflects the Channel Islands

Objectives for Public Walkways:

- Provide a continuous walkway around the Harbor and along Victoria Avenue
- Increase public access to the water
- Encourage neighborhood connectivity
- Link parks and businesses
- Widen the walkway where possible
- Increase vegetation along the walkway for improved appearance
- Maintain existing views to the water as much as possible
- Incorporate interpretive information
- Provide more seating
- Have unified site furnishings
- Long-lasting construction for durability

recommendations: public walkways

Pedestrian access is critical to bring people to the water's edge and provide opportunities for recreation such as walking and jogging. Public walkways connect destinations and nodes, and can link the Harbor with adjacent neighborhoods. The design of public walkways can also help tell the story about the Harbor and its unique qualities. The intent of the Plan is to provide safe, continuous pedestrian access throughout Channel Islands Harbor that creates a pleasant environment and enhances the visitor experience.

Design Criteria

- Continuous walkway (along the water edge as much as possible)
- ADA-compliant access to waterside promenade
- Access between parking lots and marina gangways should be as direct as possible, to minimize conflicts with pedestrians
- Use consistent paving treatment to identify publicly accessible walkways (e.g., colored concrete, decorative pavement markings)
- Walkway width should be a minimum of 10 ft; increase to 14 ft where possible (as recommended in the City of Oxnard Bicycle and Pedestrian Facilities Master Plan)
- Concrete slab, designed by registered civil engineer, with 25-year design life that considers marine environment and soil conditions
- Where possible, place benches and other furnishings on 5-ft bump-outs to keep path clear
- Pedestrian linkages (6 ft wide) from adjacent streets to waterside walkway should have similar paving treatment as waterside promenade, and have enhanced landscaping and lighting
- Minimize obstructions (signs, furniture, etc.) to views of water
- Clearly note areas to access water taxi, public docks, parks, etc.

- Provide "Distance to..." information at key points along walkway
- Remove fencing between walkway and water wherever possible; fencing installed for safety reasons should not be chain link
- Etiquette signage to reduce conflicts between pedestrians and cyclists
- Provide seating for public use (bench, seatwall, picnic table, or other table/chair set) approximately every 200 ft along waterside walkway
- Use durable site furniture (benches, etc.) that can withstand the marine environment and is consistent with publicly expressed preferences for design and appearance (see Landscape section for furniture recommendations)
- Increase landscaped areas along the waterside promenade (see Landscape section for plant palette):
- Plant groundcover or stabilizing vegetation at the top of the revetment where possible
- Provide additional plantings on the landside of the promenade that are human-scale; additional trees and shrubs should fall within the "canopy-print" of existing plants where possible to maintain existing view corridors
- Vegetation should not be so dense or continuous as to reduce the perception of safety along the walkways
- See sections on Signage and Lighting for signage and lighting design criteria that apply to public walkways

What Follows

The overall plan for the Harbor's public walkways is shown on page 15. More specific graphic design guidelines, illustrated by conceptual photocomposites, are on pages 16-23. A summary of existing conditions of the Harbor's walkways is in the appendix.

Channel Islands Harbor: Proposed Walkways

Legend

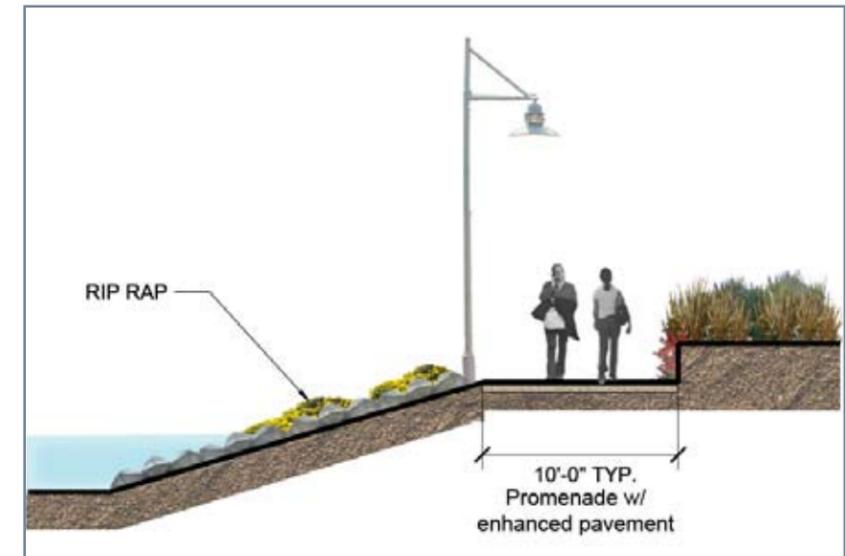
- Promenade (minimum 10 ft wide; increase to 14 ft wide where possible; existing or to be built in short term)
- Promenade (10 ft wide; to be built after 2015)
- Interim Promenade Connection
- Promenade (<10ft wide; existing or to be built in short term)
- Promenade-Street Connectors
- Water Taxi Stops
- Bench Locations
- Bike Racks
- Distance References
- Destinations



Channel Islands Harbor recommendations for public walkways

recommendations: public walkways

10-ft waterside promenade, for areas around the Harbor where topography and existing buildings preclude widening the walkway



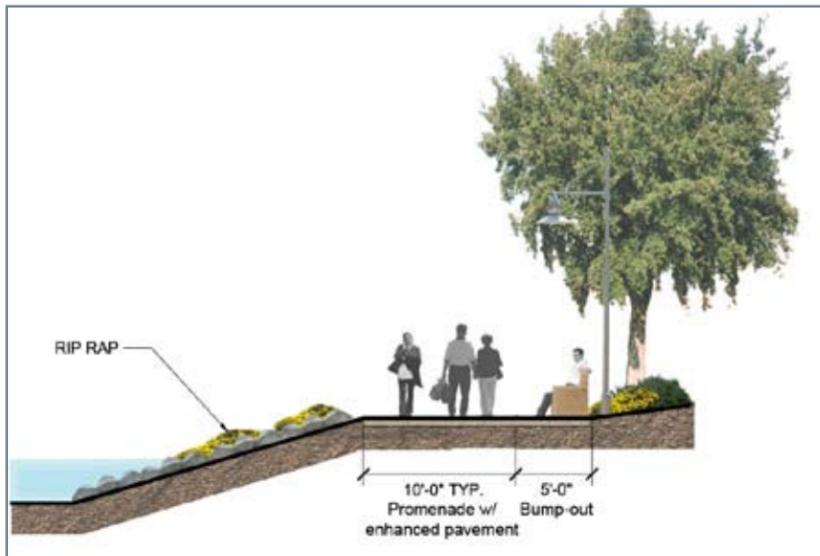
Typical section of 10-ft wide waterside promenade



Peninsula walkway - before: existing width constrained by topography



Peninsula walkway - after: furnishings can be located on revetment



Typical section of 10-ft wide waterside promenade with furniture placement



West side walkway - before: section with space for site furniture



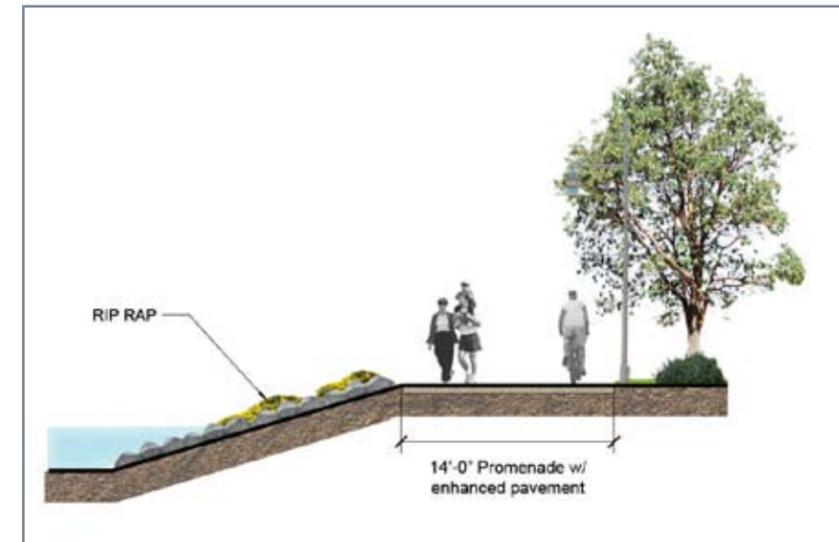
West side walkway - after: note furniture set off the main walkway

10-ft waterside promenade with room to place furniture on a bump-out adjacent to the walkway

Low seatwalls may also provide seating opportunities in some areas

recommendations: public walkways

14-ft waterside promenade, for areas around the Harbor where sufficient space exists to increase walkway width for longer distances



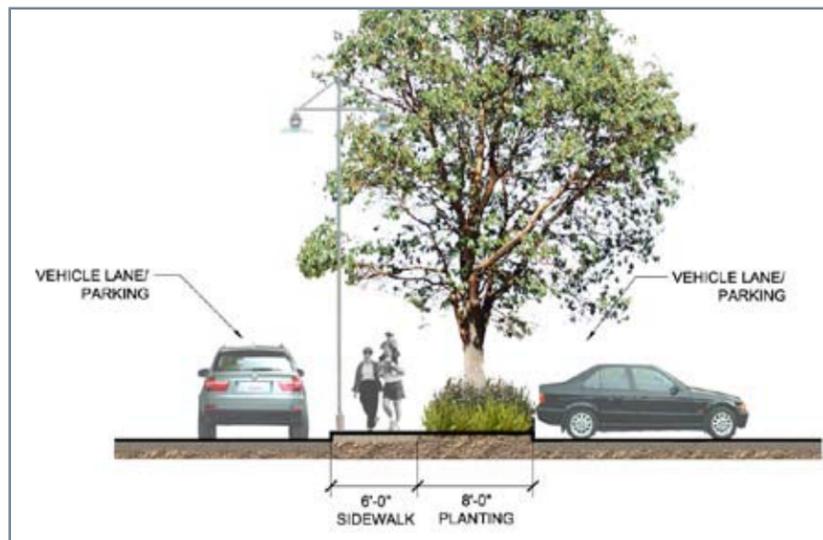
Typical section of 14-ft wide waterside promenade



Example of a location where the walkway width can be increased - before



Widened walkway - after



Typical section of promenade-street connector



Example of a location for promenade-street connector - before



Promenade-street connector - after: landscape and lights connect people to water

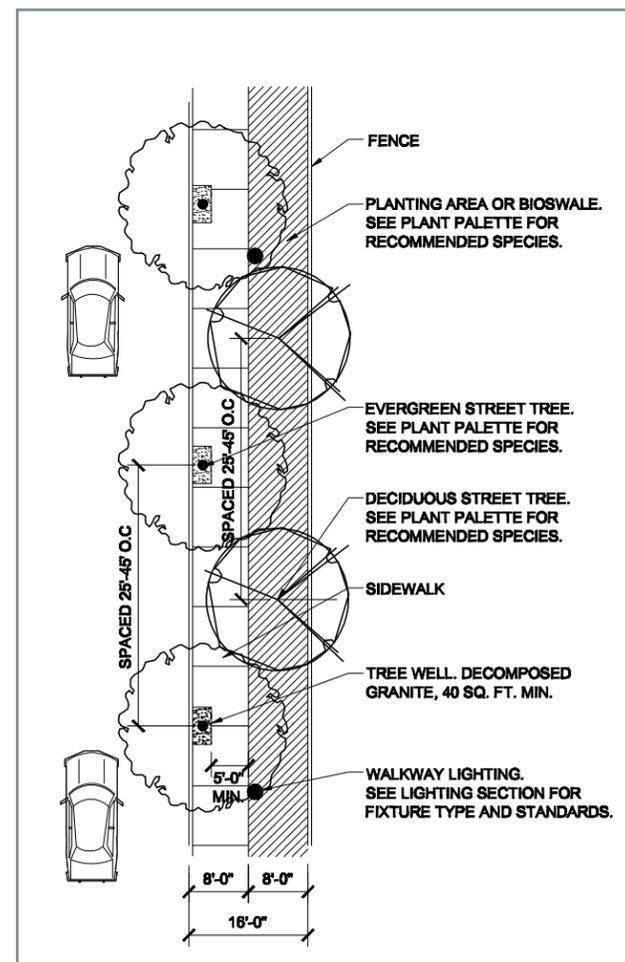
Promenade-street connectors, to facilitate pedestrian access to waterside promenade from streets adjacent to the Harbor

recommendations: public walkways

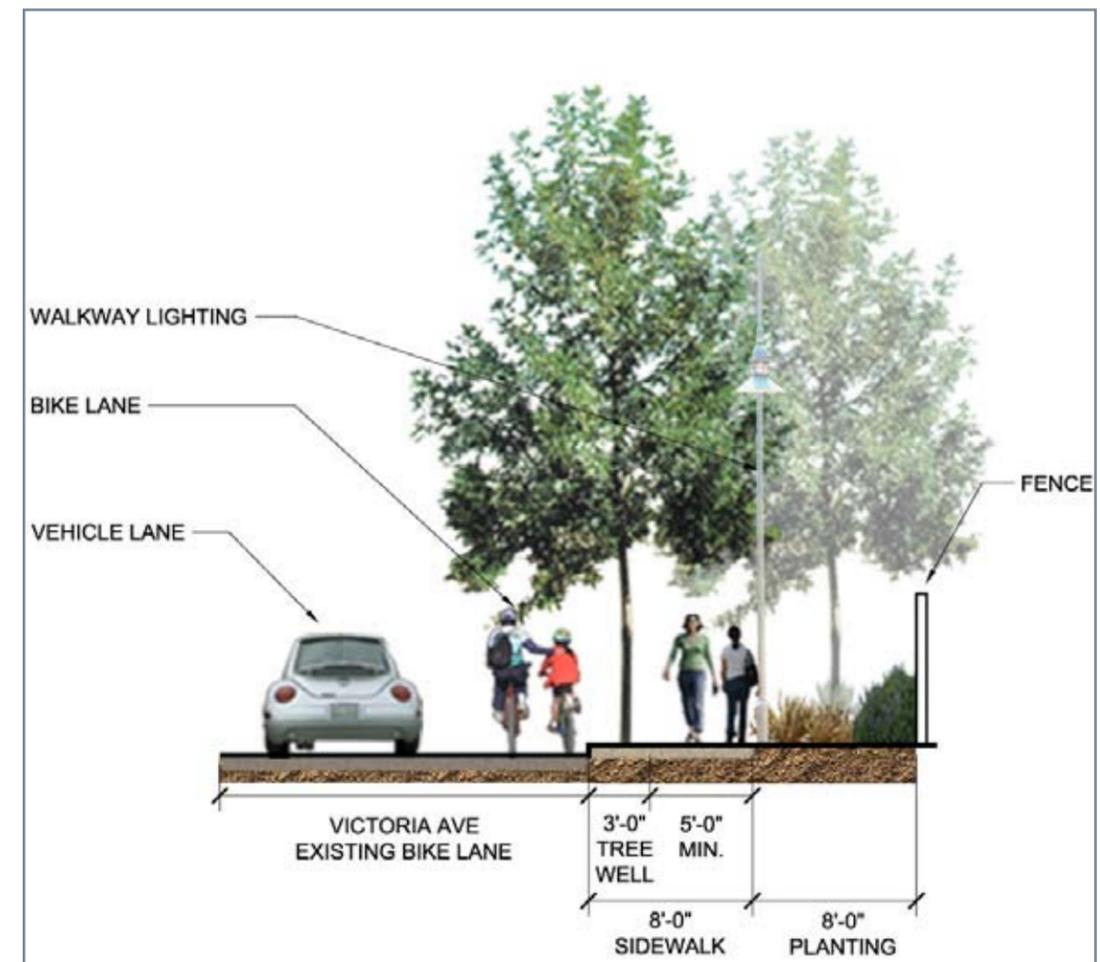
Walkway along Victoria Avenue (adjacent to east side of Channel Islands Harbor).

- Add trees and other landscaping along Victoria Avenue, including east side.
- On west side, alternate trees on either side of sidewalk.

Victoria Avenue and the adjacent right-of-way are owned by the City of Oxnard. The unimproved strip along the east side of Victoria Avenue is also owned by the City of Port Hueneme and the U.S. Navy. Improvements to the public street will be coordinated through these public agencies, and construction will meet the required specifications of these agencies.



Conceptual plan of portion of Victoria Avenue walkway



Typical section of Victoria Avenue west side walkway - note alternate street tree planting for shaded canopy



Victoria Avenue walkway - before: no improved walkway along much of the road



Victoria Avenue walkway - after: lighted sidewalk, trees, and screen plantings

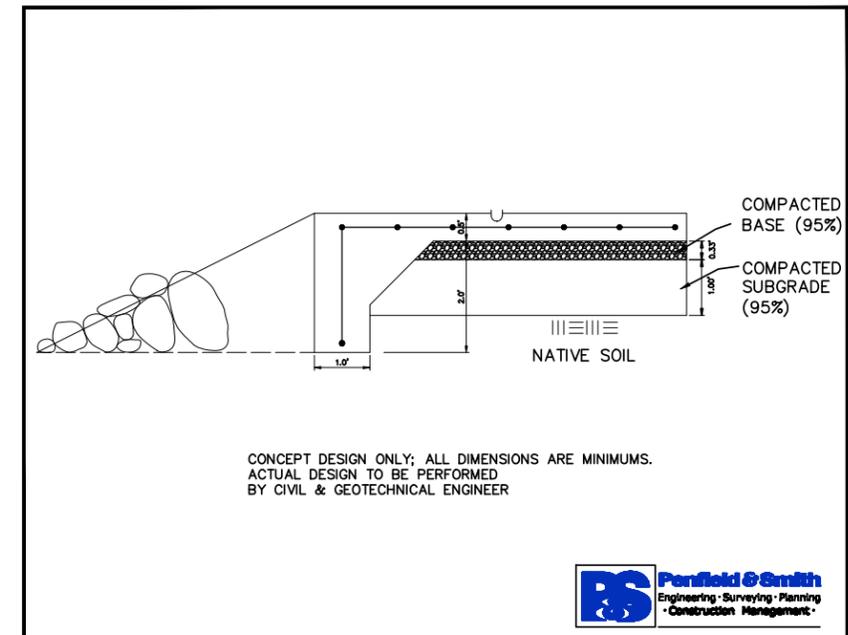
recommendations: public walkways

Typical walkway detail and guidelines

The pavement for public walkways should be constructed for aesthetics, durability, and a 25-year design life. Asphalt and concrete pavement shall be designed to accommodate reasonably foreseeable vehicle loads (e.g. H20 highway loading), and shall be specified by a California registered Civil Engineer, in consultation with a California registered Geotechnical Engineer.

For concrete walkways not exceeding 15 feet in width, along the waterfront, the design shall include the following unless approved otherwise in writing by the Harbor Director.

- Doweled expansion joints at 10-feet on center
- Crack control joints at 5-feet on center, each direction
- 4500 psi concrete strength
- Type II cement, 7½ sack minimum
- 0.40 water/cement ratio
- 15% fly ash content
- 10-day moist cure with curing compound
- Green epoxy-coated reinforcing steel 60 ksi No. 4s @ 12 inches each way
- ¾-inch chamfer on exposed edges



Conceptual section of concrete sidewalk along waterfront. Width varies from 6 ft to 14 ft. Actual thickness, reinforcement, base and subgrade preparation and concrete mix shall be determined by registered Civil and Geotechnical engineers, and shall be submitted for review to Harbor Department. Sidewalk design shall comply with City of Oxnard requirements when applicable. See corresponding text for additional guidelines.



Decorative concrete surfaces (left to right): abalone and glass; abalone; seashells.



Existing pavement near Harbor Landing highlights entries and gangways; these and similar areas can be gradually converted to decorative concrete as the walkway is rehabilitated.



Example of pavement banding to delineate a promenade. Similar banding at longer intervals can be used to identify the Harbor's public walkways.

Enhanced paving surface treatments are recommended to serve the following purposes:

- Identify walkways that are accessible to the public
- Highlight walkway entry points from businesses and promenade-street connectors
- Signify walkway segments that are adjacent to parks
- Provide an extra layer of interpretive graphics

Suggested decorative treatments include crushed abalone shell (or other seashells), recycled tumbled glass, stamped and/or colored concrete, decorative pavers, terrazzo medallions, and sandblasted concrete.

Pavement banding patterns can be used at periodic intervals along public walkways (e.g., 8-ft wide bands every 30 ft to 60 ft), and can also identify walkway entry points and gangway access areas. Bands can be formed from concrete topped with decorative aggregate, colored concrete (e.g., dark gray, for subtle contrast with light gray concrete), and decorative pavers.



Example of decorative aggregate inlay using tumbled glass and shells in concrete. This can spotlight graphic motifs described further in the Signage section.

Enhanced paving:

- Adds interest
- Identifies entry points
- Provides interpretive graphics
- Directs the public

recommendations: parks

Intent of Park Design Criteria:

- **Help unify Harbor aesthetics and identity with an overriding theme**
- **Increase park space where possible:**
 - Add to existing parks
 - Develop unimproved land
 - Create pocket parks
- **Ensure accessibility of parks**
- **Provide more opportunities for children's play**
- **Use the parks to educate visitors about the Channel Islands, the Chumash, and boating**
- **Maximize park usability through varied design and programming opportunities**

Channel Islands Harbor has valuable public parks and open space that, with additional improvements and programming, can greatly enhance the usability of the Harbor for a wide range of people. The uses can span from individuals or couples out for a quiet stroll, to families enjoying a picnic, to classroom field trips, to larger gatherings for outdoor concerts and festivals. This section briefly describes the objectives for parks in the Harbor, and proposes design concepts for the Harbor's parks.



Design Criteria

- Each of the Harbor's parks should have a different activity focus and interpretive concept, to take advantage of each park's unique features and create a system of complementary public spaces
- Each of the Harbor's three districts (West, Peninsula, East) should have a public park that provides children's play equipment
- "Contemplation" pocket parks should be located along the waterside promenade for quiet gatherings near the water
- The Harbor's parks should be accessible from public walkways
- Parks should accommodate family-friendly active and passive recreation, such as picnicking, informal play, fishing, kite-flying, and viewing Harbor activities
- Each park should provide interpretive components through such elements as signage panels, fixture embellishments, pavement markings, play equipment, art, and landscape plantings
- Design of interpretive and other features related to Chumash culture should be done in consultation with Chumash representatives
- Park landscaping should complement view corridors and screen undesirable views
- Landscape plantings and site furnishings should follow recommendations described in the conceptual plans that follow and in the Landscape section
- Park plans shall be signed by a California licensed landscape architect, and reviewed and approved by the Harbor Department
- Signs and lighting shall provide for visibility, safety, and information display while minimizing interference with boating activity and residential areas (see Signage and Lighting sections for applicable signage and lighting design criteria)

Overall Park Theme and Conceptual Park Plans

The theme for a cohesive park system in Channel Islands Harbor is inspired by the unique connection between the Harbor and the nearby Channel Islands. Each island in the chain is unique, with characteristics that only occur on that particular island (such as plants or wildlife, landforms, etc.), yet the islands remain as one group or system. As applied to parks and open space around the Harbor, individual parks may have unique features and programming, yet there is a consistent design vocabulary and connection via the promenade so that the parks function as an integrated system. Taken a step further, each of the five main parks in Channel Islands Harbor may reflect one of the five islands in Channel Islands National Park (with the understanding that there are three additional islands in the group that are not in the national park).



The association of the native Chumash with the Channel Islands is also a significant element to bring into park design, and the promenade can be imagined as the navigation paths the Chumash used to go among the islands and the mainland in their tomols (a tomol is a plank canoe built from redwood logs).

As part of the “Islands at the Harbor” framework, each park will include an entry plaza with interpretive signage, a common “signature tree” such as Catalina Ironwood or Island Oak found on the islands, and shared design characteristics such as signage, lighting, and furniture that will help identify the parks as a unified system around Channel Islands Harbor. Other aspects of the design vocabulary should remain subtle, to keep the focus on the water and the Harbor’s peaceful nature.

What Follows

The overall plan for the Harbor’s parks, plus images of existing parks, are presented on pages 26-27. Existing conditions and conceptual plans for each of the five parks in Channel Islands Harbor, illustrated by photocomposites, are shown on pages 28-53. A brief summary of existing conditions of the Harbor’s parks is presented in the appendix.

Each of the five parks in the Harbor reflects one of the islands in Channel Islands National Park:

Island	Corresponding Park
Anacapa Island	Channel View Park (“Anacapa Park”)
Santa Cruz Island	Harbor View Park (“Santa Cruz Park”)
Santa Barbara Island	Mandalay Bridge Park (“Santa Barbara Park”)
Santa Rosa Island	Peninsula Park (“Santa Rosa Park”)
San Miguel Island	Harbor East Park (“San Miguel Park”)

recommendations: parks



Channel View Park (existing conditions)



Harbor View Park (existing conditions)



Mandalay Bridge Park (existing conditions)



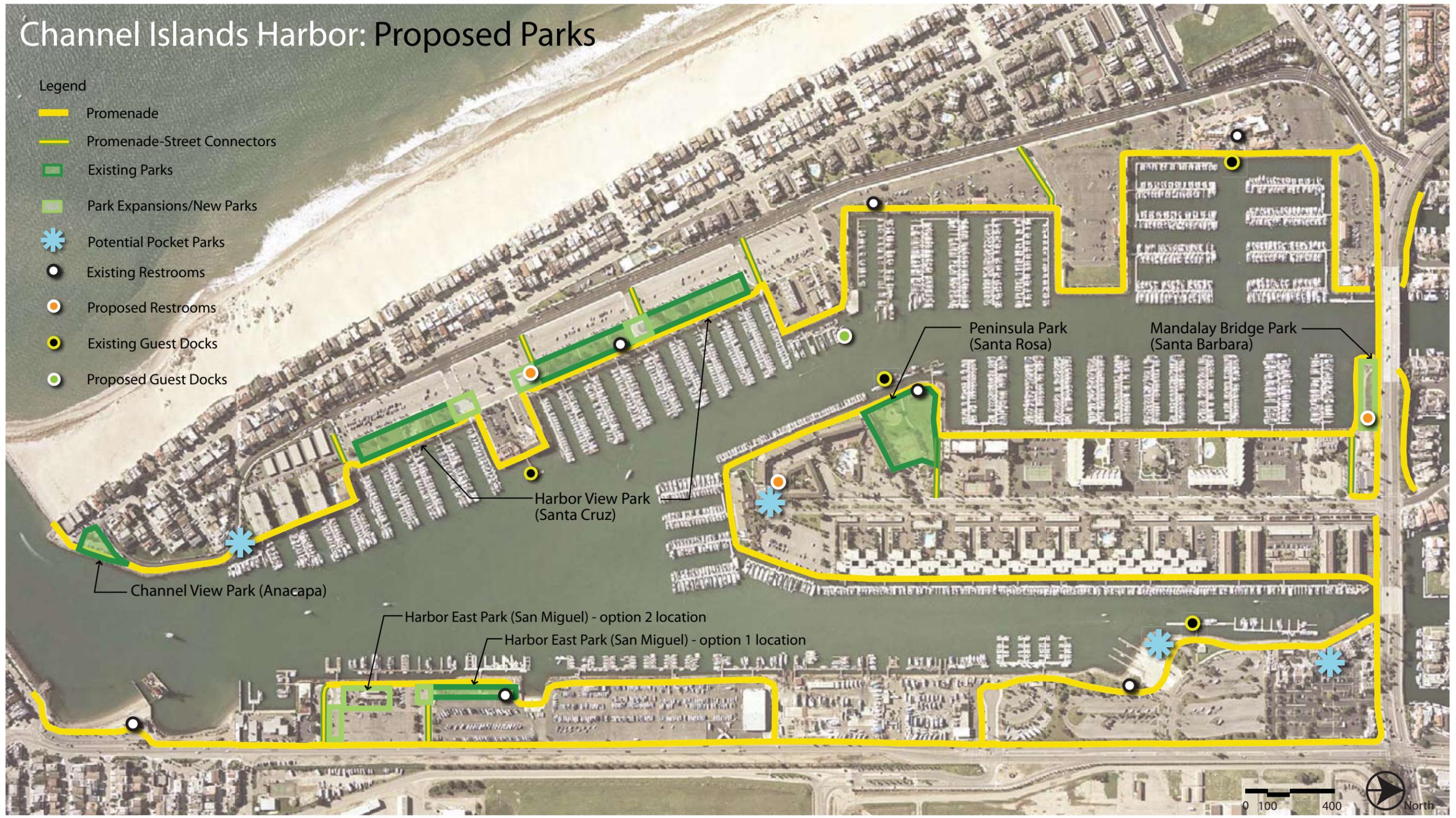
Peninsula Park (existing conditions)



Harbor East Park (existing conditions)

Channel Islands Harbor: Proposed Parks

- Legend
- Promenade
 - Promenade-Street Connectors
 - Existing Parks
 - Park Expansions/New Parks
 - Potential Pocket Parks
 - Existing Restrooms
 - Proposed Restrooms
 - Existing Guest Docks
 - Proposed Guest Docks



Overview of parks in Channel Islands Harbor

recommendations: parks

Anacapa Park

- **Overlook deck**
- **Night sky views**
- **Close to water**
- **Sweeping vistas**

The inspiration for this park comes from Anacapa Island, which is visible from the park. The table below summarizes features of the island, and how they might be reflected in the park.

Anacapa Island Features	Park Characteristics
<ul style="list-style-type: none"> • Closest island to the mainland • Lighthouse on Anacapa and Arch Rock located nearby • Anacapa Island is said to “change shape” during the day because of the fog and sun patterns • The blooming giant coreopsis and red paintbrush plants can be seen from the mainland with yellow and red flowers 	<ul style="list-style-type: none"> • Visual connection to Channel Islands • Beacon or gateway light or structures; possibly arching forms • Changing shapes and patterns with structures or light; play with sun and shade • Yellows and reds as a color palette for plants

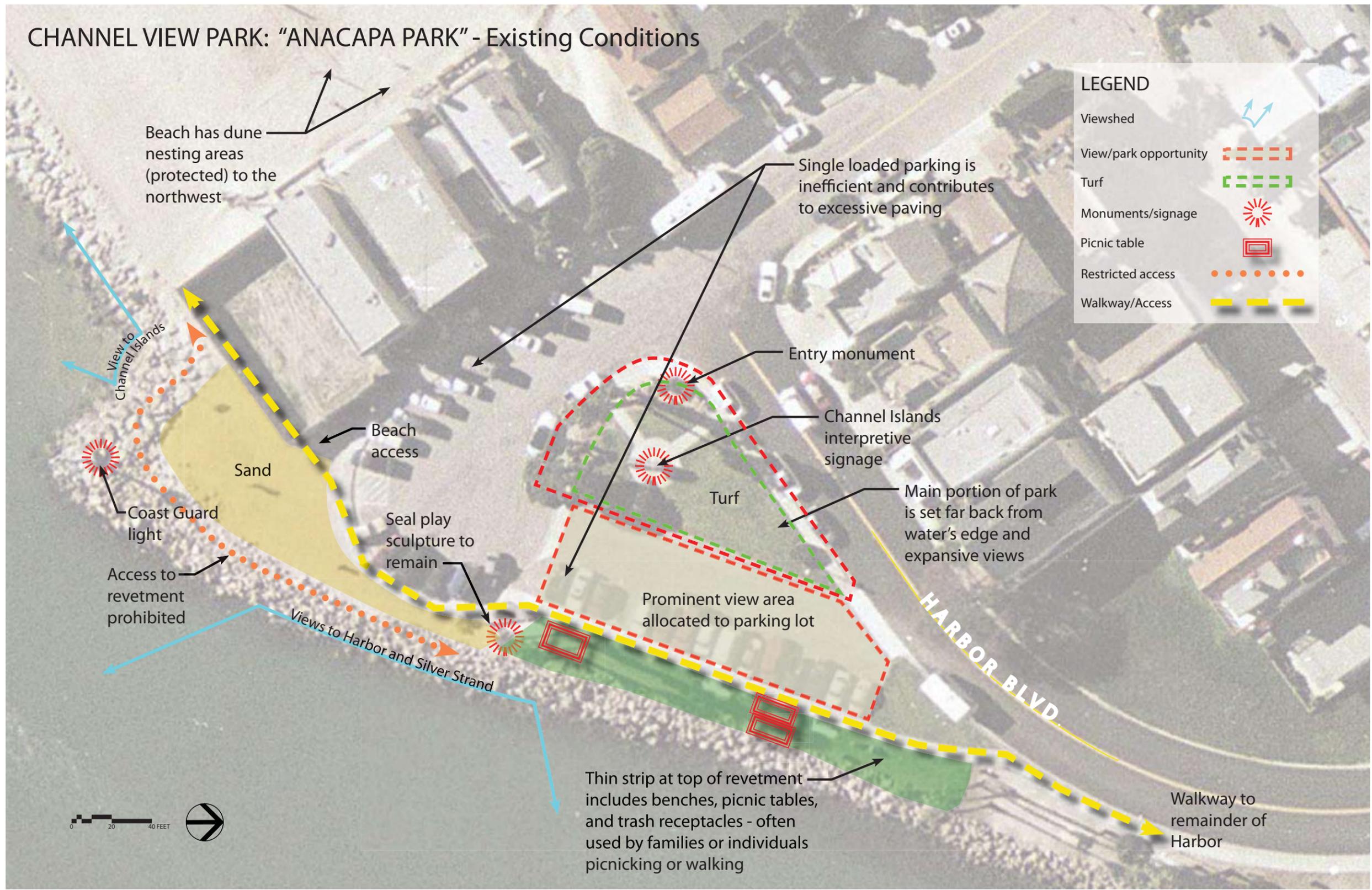


Example of viewing deck



Location of park shown by yellow marker

CHANNEL VIEW PARK: "ANACAPA PARK" - Existing Conditions



Existing conditions for Channel View Park (to be renamed Anacapa Park)

recommendations: parks

Selected plants found on Anacapa Island:

- Artemisia californica* 'Canyon Gray'
- Castilleja affinis*
- Comarostaphylis diversifolia*
- Coreopsis gigantea*
- Distichlis spicata*
- Encelia californica*
- Eriogonum arborescens*
- Eriogonum grande* var. *rubescens*
- Heteromeles arbutifolia* var. *macrocarpa*
- Huechera maxima*
- Isocoma menziesii* var. *menziesii*
- Keckiella cordifolia*
- Lavatera assurgentiflora*
- Leymus condensatus* 'Canyon Prince'
- Nasella pulchra*
- Polypodium californicum*
- Prunus ilicifolia* ssp. *lyonii*
- Quercus tomentella*
- Rhus integrifolia*
- Ribes malvaceum* 'Dancing Tassels'



Illustrative photocomposite of Anacapa Park, looking east (present condition shown in top image)

Anacapa Park: Conceptual Plan



Conceptual plan of Anacapa Park

recommendations: parks

Santa Cruz Park

- Native plant garden
- Adventure play area
- Chumash interpretive elements
- Picnics and gatherings

The inspiration for this park comes from Santa Cruz Island, the largest island in Channel Islands National Park. The table below summarizes features of the island, and how they might be reflected in the park.

Santa Cruz Island Features	Park Characteristics
<ul style="list-style-type: none"> • Largest island in the chain • Greatest diversity of plants in Channel Islands • Year-round streams and waterfalls • Site of Painted Cave, the world's largest sea cave • Two mountain ranges, steep canyons, cliffs, etc. • Had a large population of Chumash and the most villages among the islands; place for trade 	<ul style="list-style-type: none"> • Largest park in the Harbor • Native garden with endemic Channel Island plants • Water features—fountains, streams (real or abstract) • Playground—arching and cave-like spaces and shapes, places to crawl and explore • Berms and other landforms throughout the park • Accommodate “spill-over” activity from nearby Farmer’s Market and other events

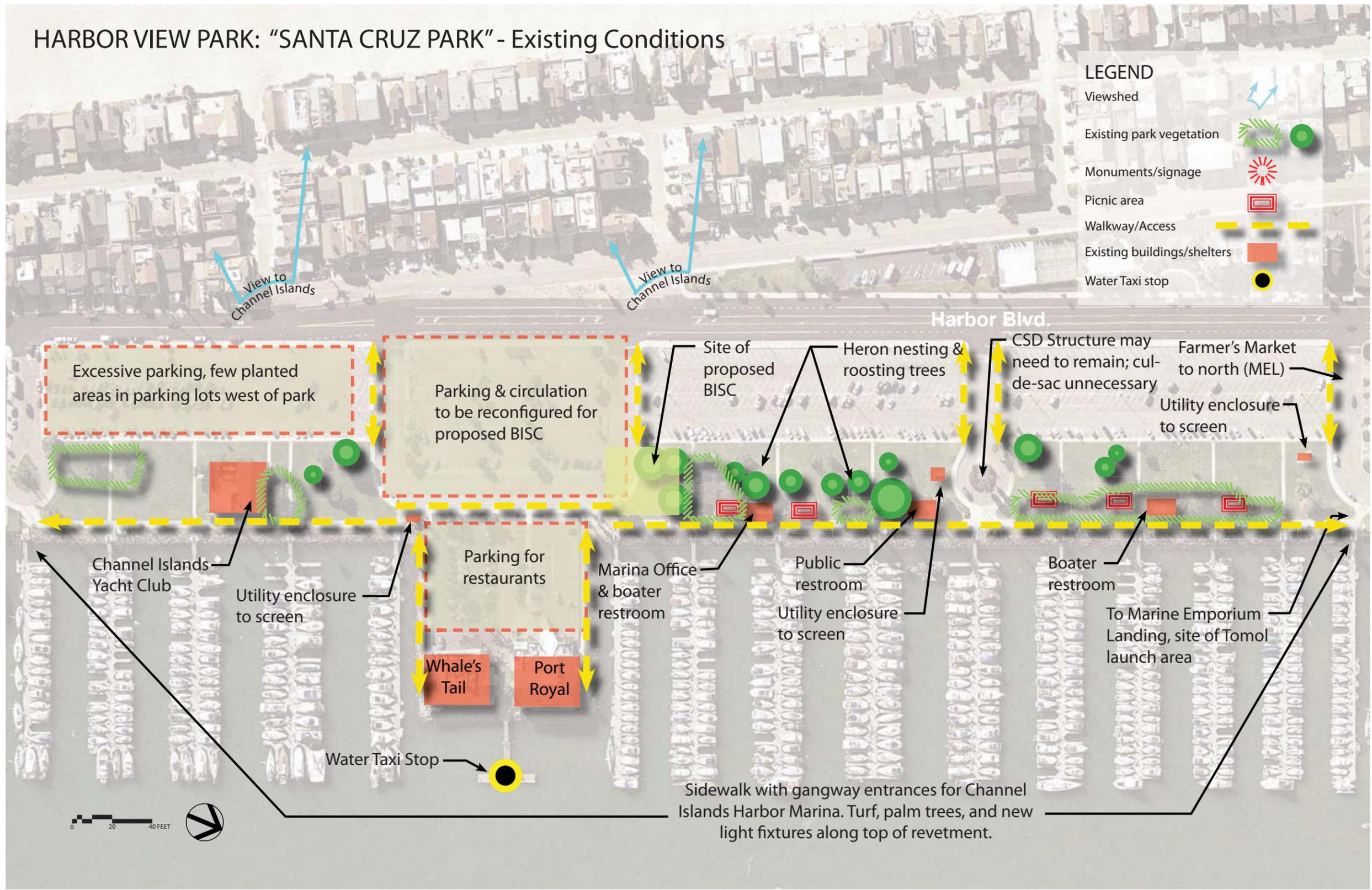


Chumash tomol



Location of park shown by yellow markers

HARBOR VIEW PARK: "SANTA CRUZ PARK" - Existing Conditions



Existing conditions for Harbor View Park (to be renamed Santa Cruz Park)

recommendations: parks

Selected plants found on Santa Cruz Island:

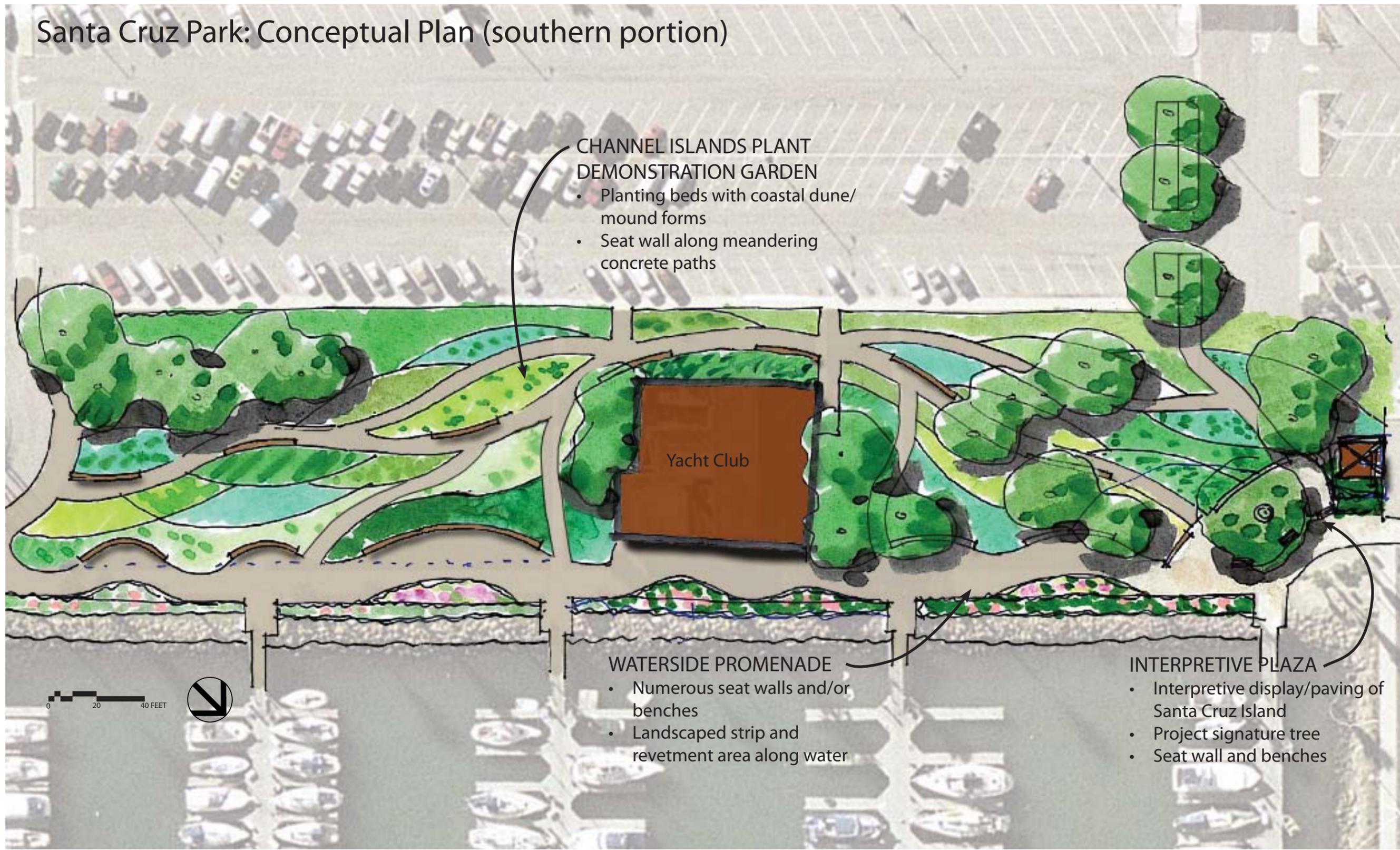
- Acer macrophyllum*
- Adenostoma fasciculatum* 'Nicolas'
- Arbutus menziesii*
- Arctostaphylos insularis* 'Canyon Sparkles'
- Artemisia californica* 'Canyon Gray'
- Camissonia cheiranthifolia*
- Castilleja affinis*
- Ceanothus arboreus*
- Comarostaphylis diversifolia*
- Coreopsis gigantea*
- Dendromecon harfordii*
- Dichondra occidentalis*
- Distichlis spicata*
- Encelia californica*
- Eriogonum arborescens*
- Eriogonum giganteum*
- Eriogonum grande var. rubescens*
- Heteromeles arbutifolia var. macrocarpa*
- Huechera maxima*
- Isocoma menziesii var. menziesii*
- Juncus patens*

continued on p. 36



Illustrative photocomposite of Santa Cruz Park, looking east toward the entry plaza (present condition shown in top image)

Santa Cruz Park: Conceptual Plan (southern portion)



CHANNEL ISLANDS PLANT DEMONSTRATION GARDEN

- Planting beds with coastal dune/mound forms
- Seat wall along meandering concrete paths

WATERSIDE PROMENADE

- Numerous seat walls and/or benches
- Landscaped strip and revetment area along water

INTERPRETIVE PLAZA

- Interpretive display/paving of Santa Cruz Island
- Project signature tree
- Seat wall and benches

Conceptual plan of Santa Cruz Park (south of Whale's Tail restaurant)

recommendations: parks

Selected plants found on Santa Cruz Island (continued from p. 34):

Keckiella cordifolia

Lepechinia fragrans

Leymus condensatus 'Canyon Prince'

Lyonothamnus floribundus ssp. asplenifolius

Mimulus cardinalis

Nasella pulchra

Pinus muricata

Polypodium californicum

Populus fremontii

Prunus ilicifolia ssp. lyonii

Quercus agrifolia

Quercus tomentella

Rhus integrifolia

Ribes malvaceum 'Dancing Tassels'

Ribes thacherianum

Rosa californica

Salix lucida ssp. lasiandra

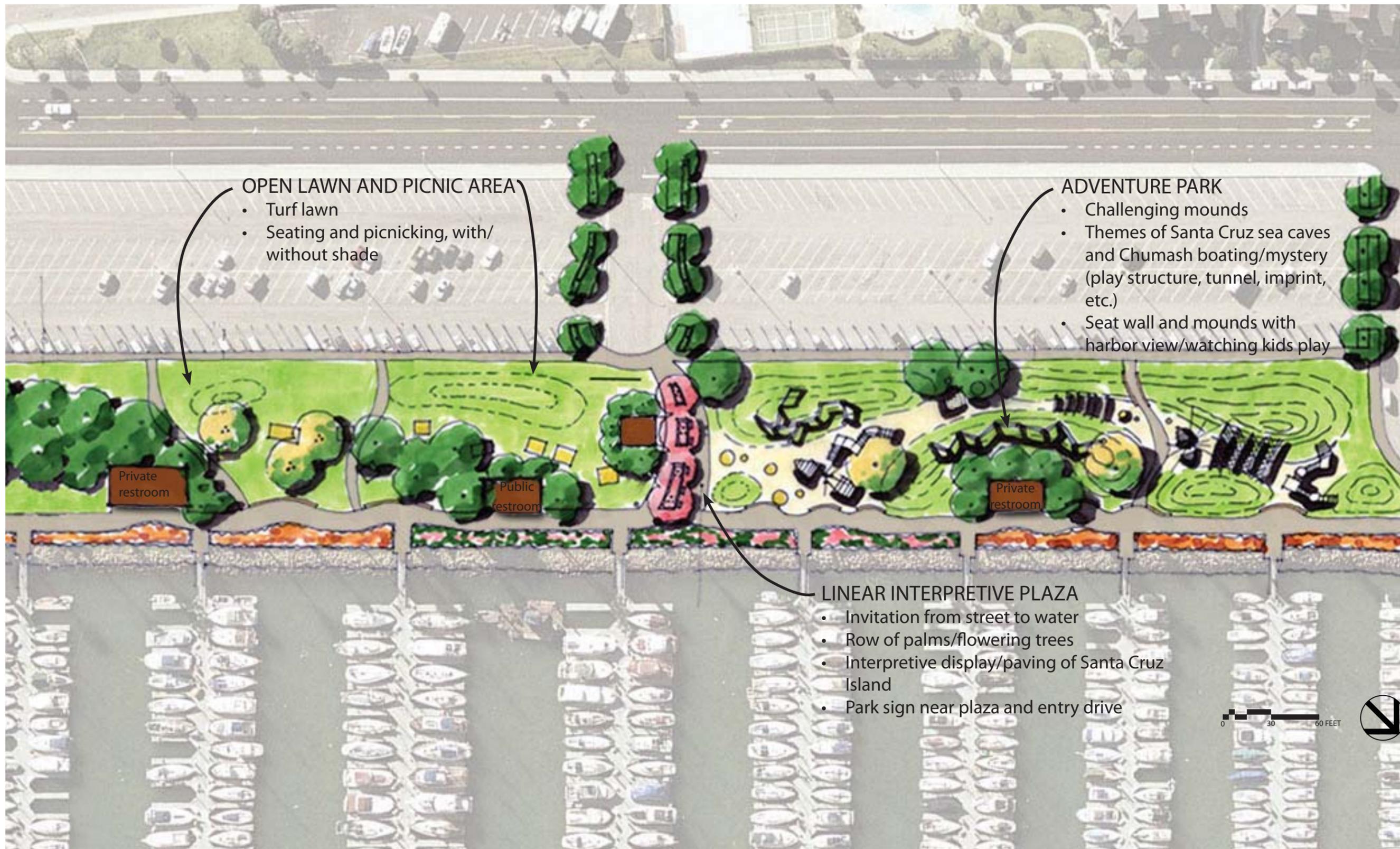
Salvia leucophylla 'Point Sal'

Sambucus mexicana

Venegasia carpesioides



Illustrative photocomposite of Santa Cruz Park, looking north from the entry plaza (present condition shown in top image)



Conceptual plan of Santa Cruz Park (north of Whale's Tail restaurant)

recommendations: parks

Santa Barbara Park

- Harbor view slope
- Interpretive deck
- Connection with Mandalay Bay
- Opportunity for public art

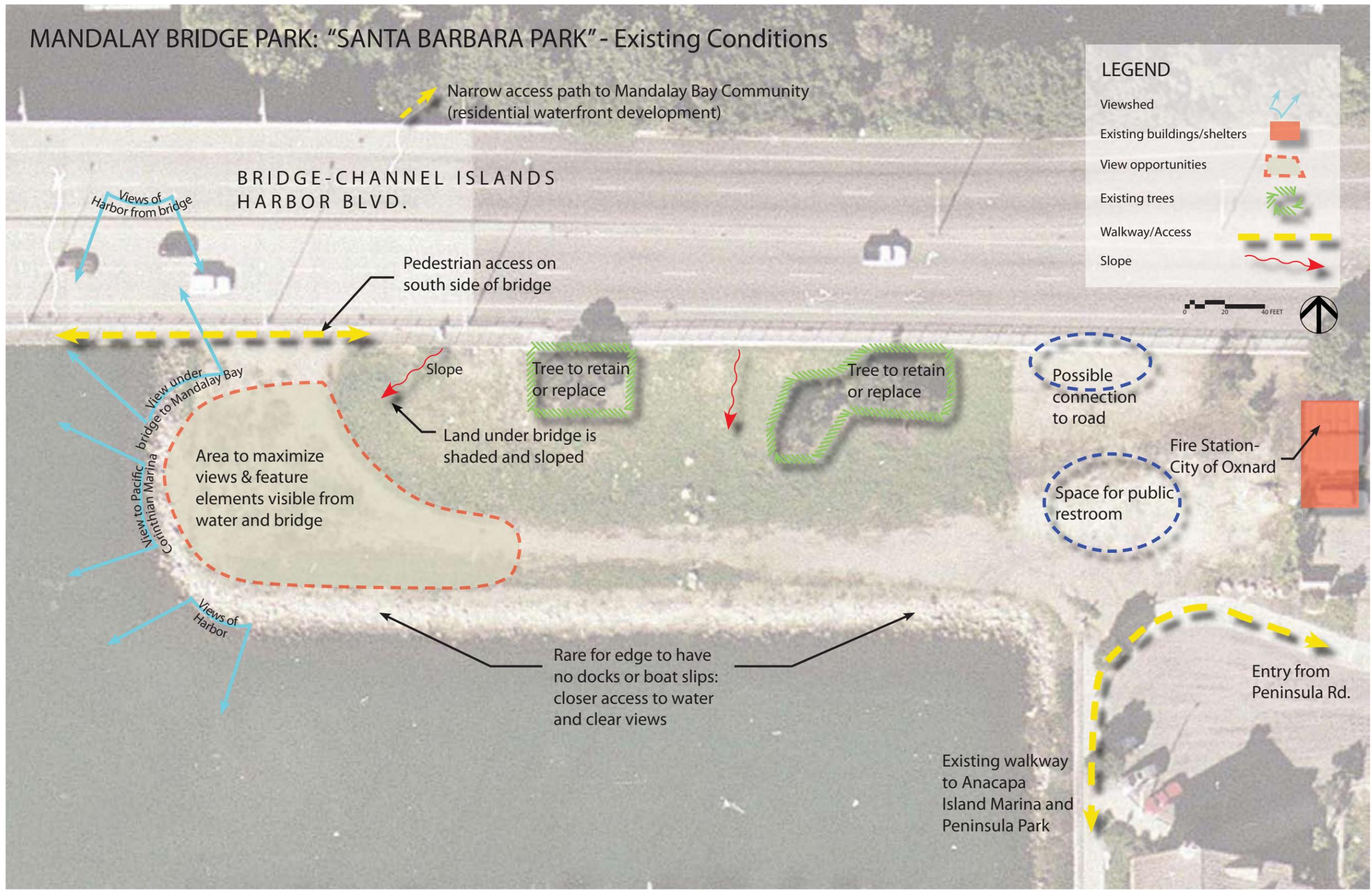
The inspiration for this park comes from Santa Barbara Island, the southernmost island in the National Park. The table below summarizes features of the island, and how they might be reflected in the Harbor park.

Santa Barbara Island Features	Park Characteristics
<ul style="list-style-type: none"> • Considered a crossroads point among the Channel Islands for people and animals • Numerous overlooks along the cliffs that enable viewing the ocean or the wildlife below • Place for wildlife watching and diving in the rich kelp forests off the coast 	<ul style="list-style-type: none"> • Place of convergence, hub; connection from Mandalay Bay; visible from bridge • Overlooks, deck structure near water, terrace seating on slope below bridge along Channel Islands Blvd. • Public art—representing wildlife, water, or kelp forests



Location of park shown by yellow marker

MANDALAY BRIDGE PARK: "SANTA BARBARA PARK" - Existing Conditions



Existing conditions for Mandalay Bridge Park (to be renamed Santa Barbara Park)

Selected plants found on Santa Barbara Island:

Artemisia californica 'Canyon Gray'

Coreopsis gigantea

Nasella pulchra

Encelia californica

Polypodium californicum

Camissonia cheiranthifolia

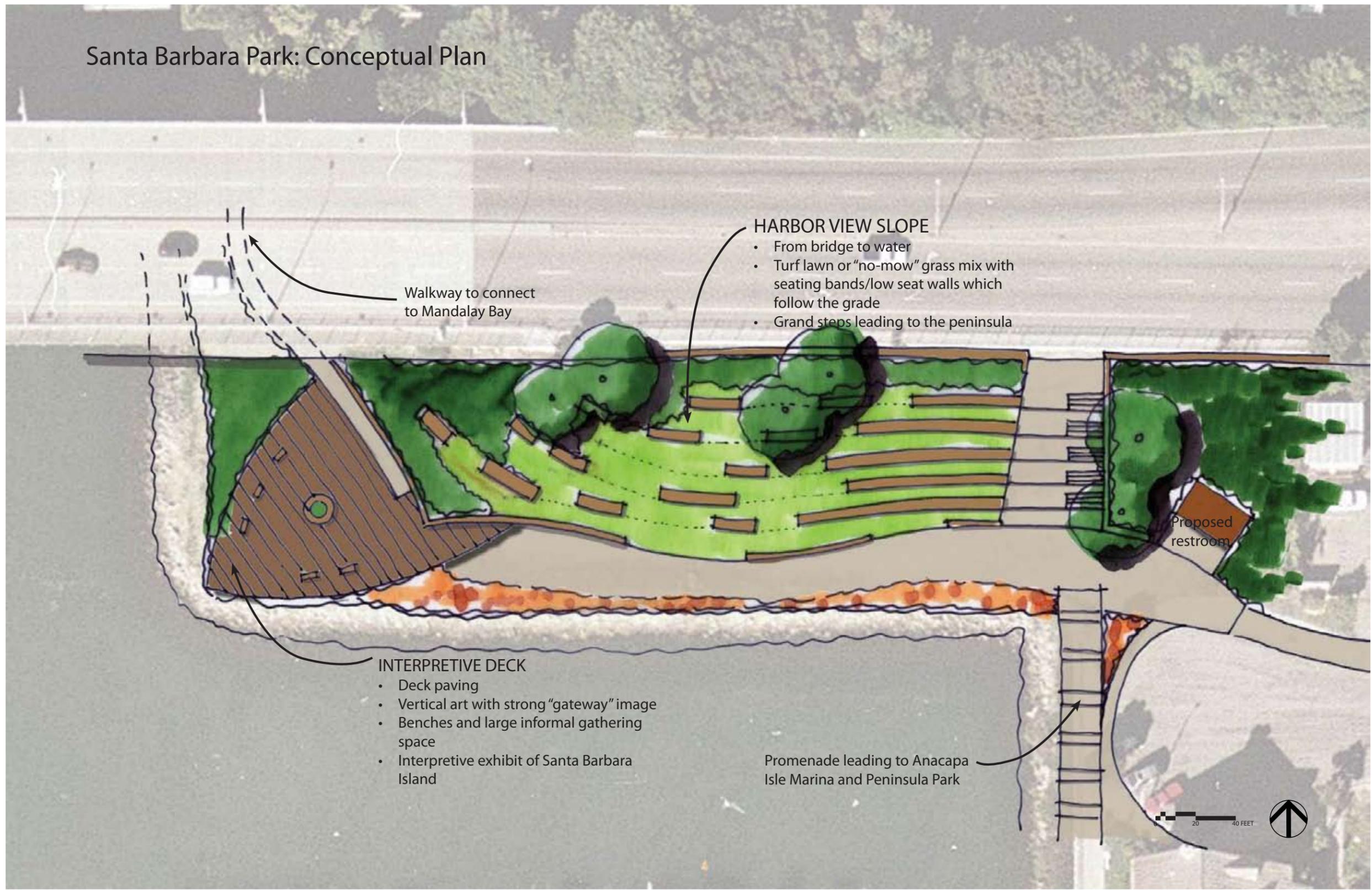
Eriophyllum nevinii

recommendations: parks



Illustrative photocomposite of Santa Barbara Park, looking from the east entry (present condition shown in top image)

Santa Barbara Park: Conceptual Plan



Conceptual plan of Santa Barbara Park

recommendations: parks

Santa Rosa Park

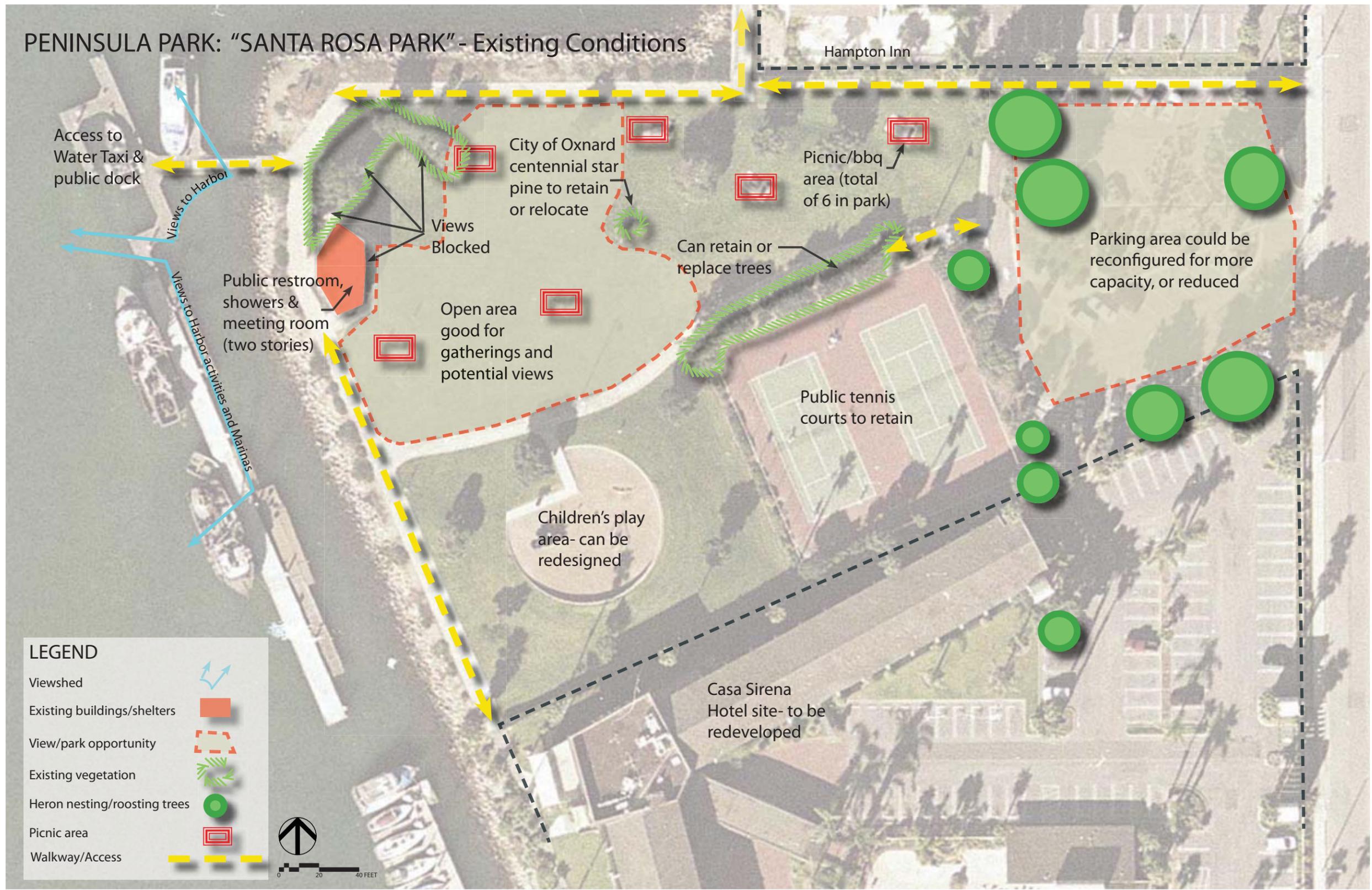
- Open event and performance space
- Enclosed tree grove
- Reconfigured play area
- Open views to Harbor

The inspiration for this park comes from Santa Rosa Island, a large island with unique vegetation. The table below summarizes features of the island, and how they might be reflected in the park.

Santa Rosa Island Features	Park Characteristics
<ul style="list-style-type: none"> • Second largest island with a number of Chumash villages and associated human activity; central ridge • Santa Rosa Island has a range of coastline morphology, from gentle slopes to sheer cliffs • Torrey Pines occur naturally on this island (of only two spots in California); 6 other plant species are found only on Santa Rosa Island 	<ul style="list-style-type: none"> • Location near hotels good for performance venue; amphitheater with seating sloped down toward stage; use of stones or paving strips • Play structure with varying heights of play equipment, natural materials • Include Torrey Pines in the plant palette plus 6 Santa Rosa endemic plant species in this park



Location of park shown by yellow marker



Existing conditions for Peninsula Park (to be renamed Santa Rosa Park)

recommendations: parks

Selected plants found on Santa Rosa Island:

<i>Adenostoma fasciculatum</i> 'Nicolas'	<i>Juncus patens</i>
<i>Artemisia californica</i> 'Canyon Gray'	<i>Keckiella cordifolia</i>
<i>Camissonia cheiranthifolia</i>	<i>Lepechinia fragrans</i>
<i>Carex pansa</i>	<i>Leymus condensatus</i> 'Canyon Prince'
<i>Carex praegracilis</i>	<i>Lupinus chamissonis</i>
<i>Castilleja affinis</i>	<i>Lyonothamnus floribundus</i> ssp. <i>asplenifolius</i>
<i>Ceanothus arboreus</i>	<i>Nasella pulchra</i>
<i>Comarostaphylis diversifolia</i>	<i>Pinus muricata</i>
<i>Coreopsis gigantea</i>	<i>Pinus torreyana</i>
<i>Dendromecon harfordii</i>	<i>Polypodium californicum</i>
<i>Dichondra occidentalis</i>	<i>Prunus ilicifolia</i> ssp. <i>lyonii</i>
<i>Distichlis spicata</i>	<i>Quercus agrifolia</i>
<i>Encelia californica</i>	<i>Quercus tomentella</i>
<i>Eriogonum arborescens</i>	<i>Rhus integrifolia</i>
<i>Eriogonum cinereum</i>	<i>Ribes malvaceum</i> 'Dancing Tassels'
<i>Eriogonum grande</i> var. <i>rubescens</i>	<i>Rosa californica</i>
<i>Heteromeles arbutifolia</i> var. <i>macrocarpa</i>	<i>Salvia brandegei</i>
<i>Huechera maxima</i>	<i>Sambucus mexicana</i>
<i>Isocoma menziesii</i> var. <i>menziesii</i>	<i>Venegasia carpesioides</i>



Illustrative photocomposite of Santa Rosa Park, looking west toward stage area (present condition shown in top image)



Conceptual plan of Santa Rosa Park

recommendations: parks

San Miguel Park

- Harbor views from overlook deck, seat walls, and picnic tables
- “Windswept” landscape plantings
- Visual connection to Victoria Ave.
- Shipwreck and treasure themed play area

The inspiration for this park comes from San Miguel Island, which is associated with historic and prehistoric remnants of shipwrecks and plant fossils. The table below summarizes features of the island, and how they might be reflected in the park.

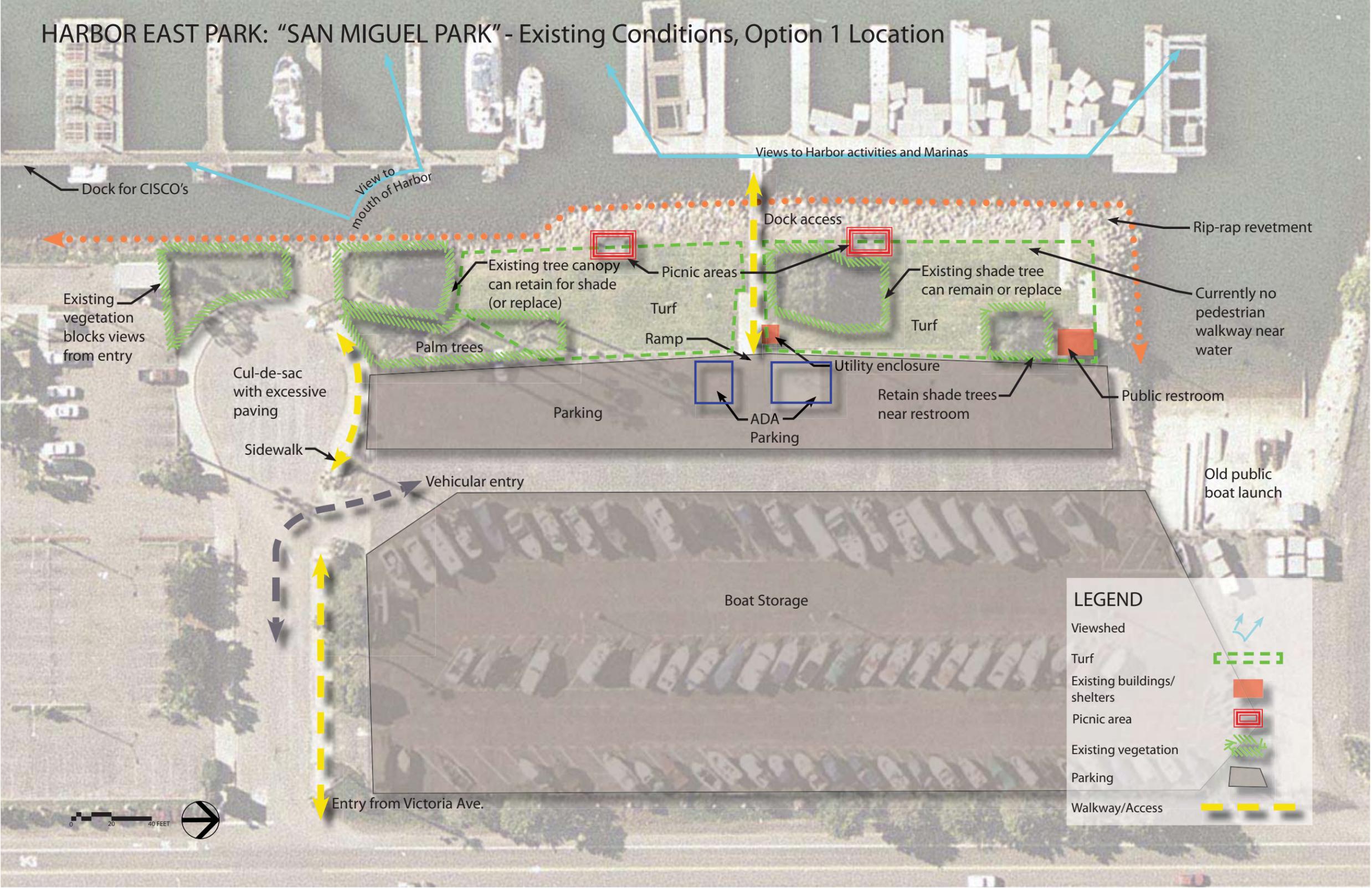
San Miguel Island Features	Park Characteristics
<ul style="list-style-type: none"> • San Miguel Island has a windswept landscape • Numerous sand castings can be found on San Miguel Island; “Caliche Forest” • Shipwrecks have occurred off of the coast of San Miguel Island, more so than other islands 	<ul style="list-style-type: none"> • “Windswept” appearance, using grasses and other coastal, picturesque plants • Imprints in the promenade and plaza pavement representing fossils • Ship-like structures for play; an overlook pier mimicking the mast or sails of a ship



Bow of the shipwreck "Comet" that was once buried in sand



Location Option 1 of park shown by yellow marker



Existing conditions for Harbor East Park (to be renamed San Miguel Park), with the Option 1 location at site of current park between CISCO's and the old public boat launch area

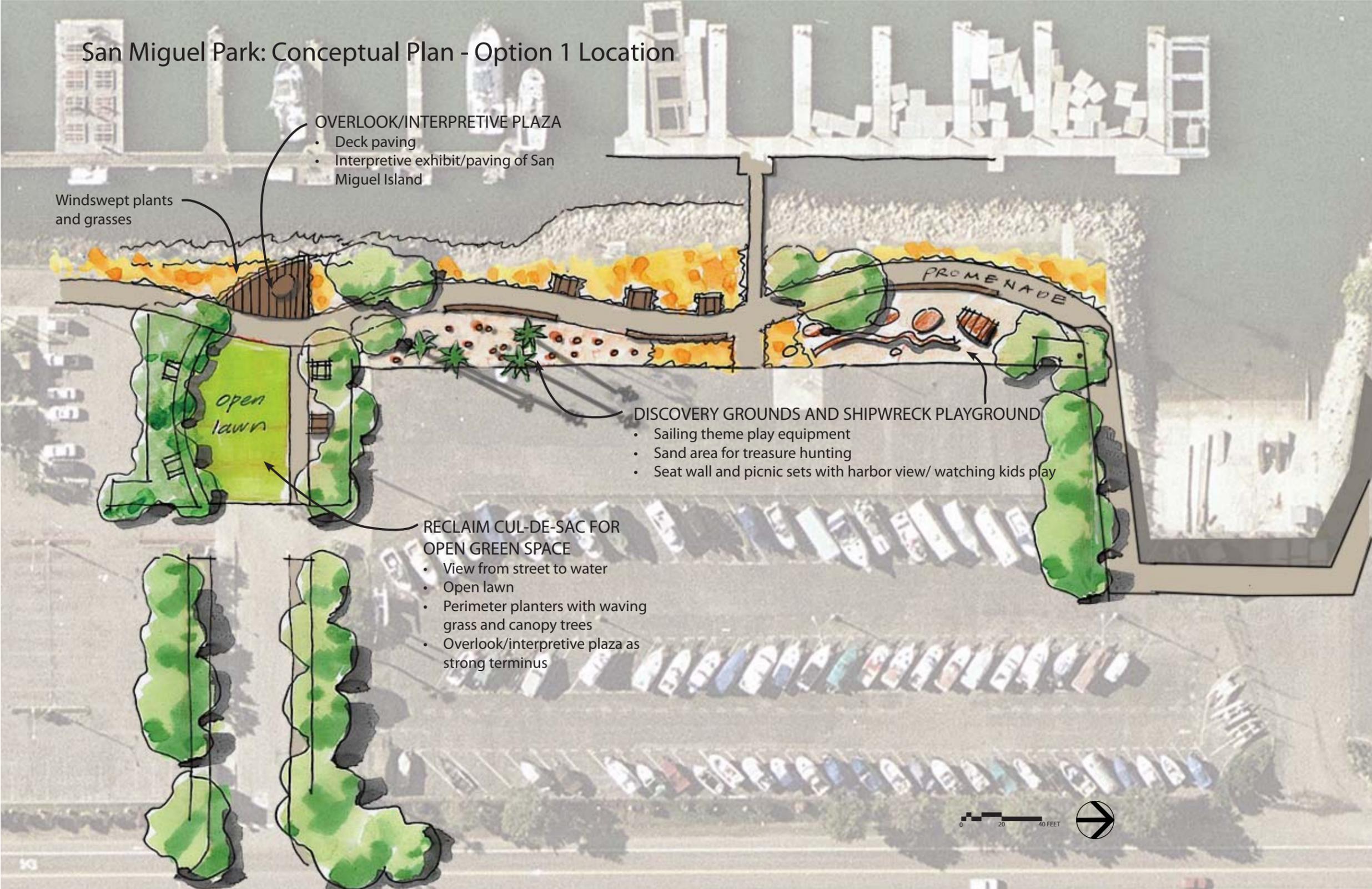
recommendations: parks

Selected plants found on San Miguel Island:

- Artemisia californica* 'Canyon Gray'
- Camissonia cheiranthifolia*
- Carex pansa*
- Carex praegracilis*
- Castilleja affinis*
- Coreopsis gigantea*
- Dichondra occidentalis*
- Distichlis spicata*
- Eriogonum grande* var. *rubescens*
- Heteromeles arbutifolia* var. *macrocarpa*
- Isocoma menziesii* var. *menziesii*
- Juncus patens*
- Lavatera assurgentiflora*
- Leymus condensatus* 'Canyon Prince'
- Lupinus chamissonis*
- Nasella pulchra*
- Rhus integrifolia*



Illustrative photocomposite of San Miguel Park (Option 1 location), looking west toward interpretive plaza (present condition shown in top image)



Conceptual plan of San Miguel Park, Option 1 location

recommendations: parks

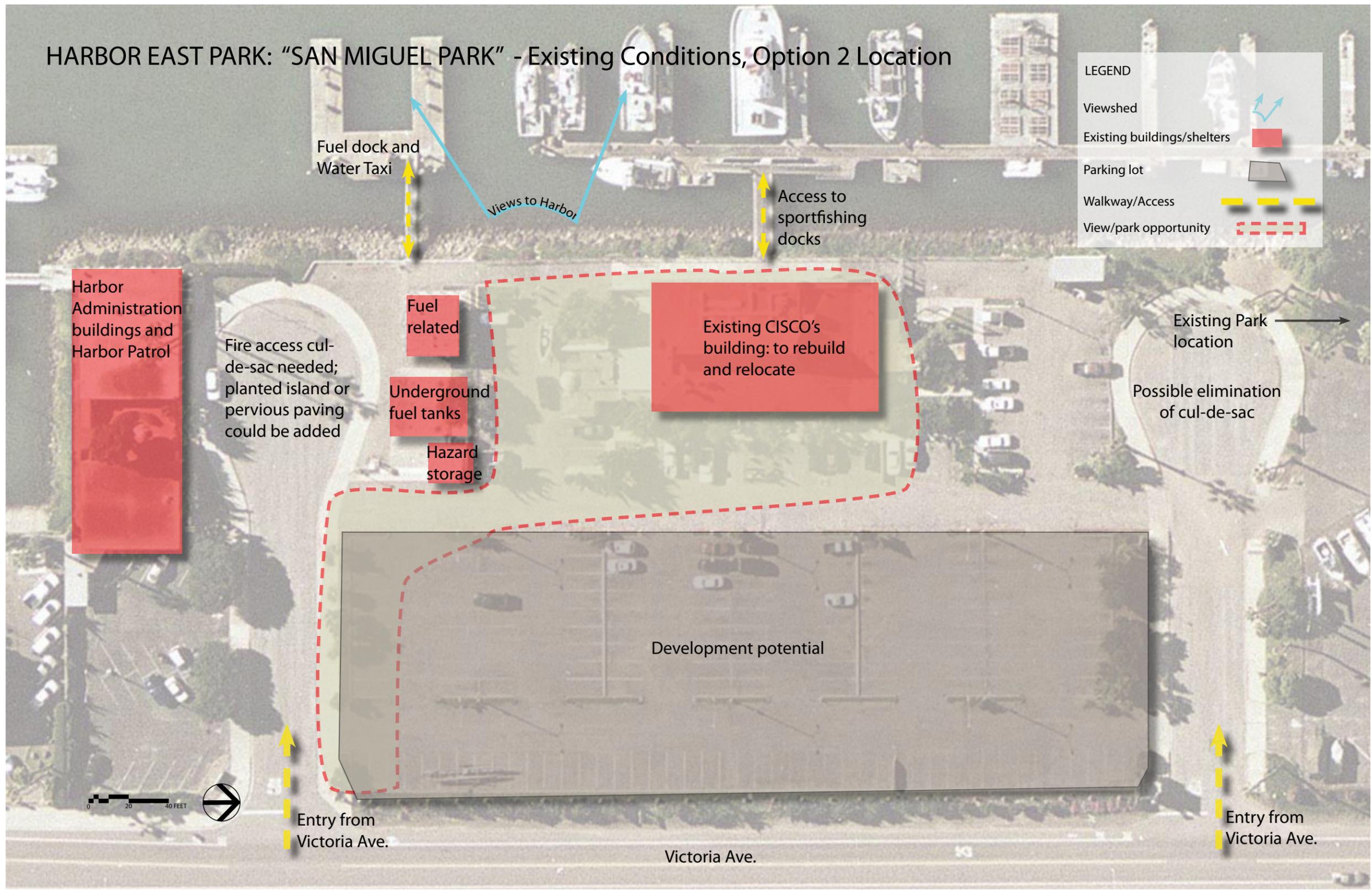
San Miguel Park: Location Option 2

An alternative location for San Miguel Park exists to the south of the current Harbor East Park, and would occupy a portion of the CISCO's site and parking lot. This alternative would have more green space nearer Victoria Avenue, and its L-shape would allow part of the park to extend along the water's edge. It assumes that the CISCO's facility would be rebuilt just to the north, with the opportunity of having a shared public restroom.



Location Option 2 of park shown by yellow marker

HARBOR EAST PARK: "SAN MIGUEL PARK" - Existing Conditions, Option 2 Location



Existing conditions for the Option 2 location of San Miguel Park), extending to the south of CISCO's

recommendations: parks

Selected plants found on San Miguel Island:

- Artemisia californica* 'Canyon Gray'
- Camissonia cheiranthifolia*
- Carex pansa*
- Carex praegracilis*
- Castilleja affinis*
- Coreopsis gigantea*
- Dichondra occidentalis*
- Distichlis spicata*
- Eriogonum grande* var. *rubescens*
- Heteromeles arbutifolia* var. *macrocarpa*
- Isocoma menziesii* var. *menziesii*
- Juncus patens*
- Lavatera assurgentiflora*
- Leymus condensatus* 'Canyon Prince'
- Lupinus chamissonis*
- Nasella pulchra*
- Rhus integrifolia*



Illustrative photocomposite of San Miguel Park (Option 2 location), looking west from Victoria Avenue (present condition shown in top image)

San Miguel Park: Conceptual Plan- Option 2 Location



Windswept plants and grasses

Water Taxi

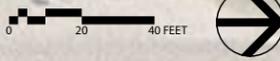
Screening Trees

TANK

NEW DEVELOPMENT

DISCOVERY GROUNDS AND SHIPWRECK PLAYGROUND
* Sailing theme play equipment
* Sand area for treasure hunting
* Interpretive Board/Paving of San Miguel Island

OPEN GREEN SPACE
* Turf Mounds mimicking the landscape of the Channel Islands
* Perimeter planters with waving grass and canopy trees



Conceptual plan of San Miguel Park, Option 2 location

recommendations: parks

Pocket Parks

In addition to the five parks in Channel Islands Harbor, a series of small pocket parks is recommended to provide for quiet enjoyment of harbor views. These parks should be immediately adjacent to the promenade. Several potential locations have been identified, including in redevelopment areas at the end of the peninsula and at Fisherman's Wharf (exact locations of pocket parks in these areas will depend on final site plans for these areas).

Pocket parks are intended to be small green nodes along the promenade, sited to take advantage of views and proximity to the water. Each should have seating (including one or two picnic tables) and a trash receptacle. Depending on a park's size and location, it may also have interpretive features.



Views to the water can be enjoyed from the west end of the peninsula



Existing view area near public boat launch can be used as a pocket park



Location of potential pocket parks shown by yellow markers



Conceptual plan of a pocket park on the Harbor's west side

recommendations: landscape

Landscape Objectives:

- Increase landscaping, especially along walkways and in parking lots
- Add native, drought-tolerant, lower-maintenance plant material
- Minimize disturbance of existing heron nesting and roosting habitat
- Provide site furniture that enhances Harbor identity and helps visitors enjoy the Harbor
- Utilize landscaped areas for stormwater filtration
- Complement signage and lighting for the public realm

This section focuses on the Harbor's plants and site furniture. Plants help determine Harbor aesthetics, define outdoor spaces, frame or screen views, provide shade and habitat, and can help filter stormwater runoff. Site furniture, such as benches and drinking fountains, are not only utilitarian, but they help unify outdoor spaces and contribute to site identity. The intent of these criteria for the landscape is to create inviting outdoor spaces that respect the Harbor's natural environment and reflect the Channel Islands.

Design Criteria

- Select plant species that primarily are native to coastal southern California and, where possible, the Channel Islands.
- Use plants to: provide shade; frame desirable views and screen undesirable views; improve the appearance of public spaces (e.g., parks, walkways, and parking lots); improve water quality of runoff from parking lots.
- Select plants to reduce irrigation, fertilizer, and maintenance needs.
- Select plants of the right size and appropriate characteristics for different areas around the Harbor.
- Provide view corridors into the Harbor from surrounding areas.
- Landscape plans shall be signed by a California licensed landscape architect, and reviewed and approved by the Harbor Department.
- All landscaped areas, including planters and pots, should be fully irrigated with automatic, permanent, underground irrigation systems. A possible exception to this would be temporary (above-ground) irrigation to establish vegetation on revetment areas.
- Irrigate with reclaimed water where possible.
- The irrigation system should use weather sensor-based controllers

to minimize excessive water application and to help identify maintenance needs.

- Irrigation should be designed by a California licensed landscape architect or Certified Irrigation Designer to maximize irrigation efficiency and for compatibility with the plant palette. Irrigation plans shall be reviewed and approved by the Harbor Department.
- Irrigation runoff should be minimized, avoiding extensive runoff that would drain across pavements into the storm drainage system and into the Harbor. To the extent possible, all irrigation runoff should be contained in planting areas, allowing the water to percolate into the soils.
- Leaseholds shall implement an irrigation maintenance program.

What Follows

The landscape plan for Channel Islands Harbor is presented on page 57. It highlights important views, and shows where trees might be located. Pages 58-69 present recommended plant species for Channel Islands Harbor. The lists are grouped by plant size category, and the possible use(s) of each species is noted. In addition, major characteristics such as light and moisture requirements are included. If this plant palette is used, Channel Islands Harbor will be very unique in having plant material that occurs naturally in California and Baja California. Significantly, much of the plant palette draws from the flora of the Channel Islands. Recommended seating, tables, bicycle racks, trash receptacles, and drinking fountains are shown on pages 70-73. These recommendations were derived from input from public workshops as well the Harbor Department.

Channel Islands Harbor: Proposed Landscape Plan



recommendations: landscape

Plant Palette

- Large trees
- Medium trees



Acer macrophyllum



Alnus rhombifolia



Arbutus menziesii



Lyonothamnus floribundus ssp. *asplenifolius*



Pinus muricata



Pinus torreyana



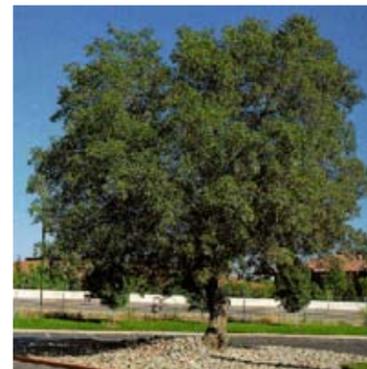
Platanus racemosa



Populus fremontii



Prunus ilicifolia ssp. *lyonii*



Quercus agrifolia



Quercus tomentella



Umbellularia californica

Latin name	Common name	Native to...	Form	height (ft)	spread (ft)	light level			moisture				uses					
						full sun	part shade	full shade	low	moderate	regular	moist	parks	promenade	revetment or slope	streetscape	bioswale	screening
Large trees																		
<i>Cupressus macrocarpa</i>	Monterey Cypress	California	evergreen conifer	40+	30-40+	*			*				*					
<i>Pinus muricata</i>	Bishop Pine	Channel Isl.	evergreen conifer	50	20+	*			*				*					
<i>Pinus torreyana</i>	Torrey Pine	California	evergreen conifer	40-70	35+	*			*				*					
<i>Platanus racemosa</i>	California Sycamore	California	deciduous tree	40-80	30+	*				*	*		*			*	*	
<i>Populus fremontii</i>	Fremont Cottonwood	California	deciduous tree	30-50+	35+	*						*					*	
<i>Quercus tomentella</i>	Island Oak	Channel Isl.	broadleaf evergreen tree	30-60	35+	*			*	*			*			*		
<i>Umbellularia californica</i>	California Bay Laurel	California	broadleaf evergreen tree	30-80	25+	*	*		*	*			*			*	*	*
Medium trees																		
<i>Acer macrophyllum</i>	Big Leaf Maple	California	deciduous tree	10-40+	20+	*	*				*						*	
<i>Alnus rhombifolia</i>	White Alder	California	deciduous tree	10-30	20	*	*	*			*	*					*	
<i>Arbutus menziesii</i>	Madrone	California	broadleaf evergreen tree	40+	30+	*	*			*			*	*		*		
<i>Lyonothamnus floribundus ssp. asplenifolius</i>	Fern-leaved Catalina Ironwood	California	broadleaf evergreen tree	20-35	15	*			*				*					*
<i>Metrosideros excelsus</i>	New Zealand Christmas Tree	New Zealand	broadleaf evergreen tree	30+	30+	*	*			*								
<i>Myrica californica</i>	Pacific Wax Myrtle	California	broadleaf evergreen tree	10-25	10+	*	*			*			*	*			*	*
<i>Prunus ilicifolia ssp. lyonii</i>	Catalina Cherry	Channel Isl.	broadleaf evergr. tree/shrub	10-40	15	*			*	*			*					*
<i>Quercus agrifolia</i>	Coast Live Oak	California	broadleaf evergreen tree	20-40	35+	*			*				*					*
<i>Salix lucida ssp. lasiandra</i>	Lance-leaf Willow	California	deciduous tree	20-30	20+	*	*					*					*	

recommendations: landscape

Plant Palette

- Small trees and tree-like shrubs
- Palm trees
- Large shrubs



Arctostaphylos catalinae



Arctostaphylos glandulosa



Brahea edulis



Calycanthus occidentalis



Ceanothus arboreus



Ceanothus impressus



Comarostaphylos diversifolia



Dendromecon harfordii



Heteromeles arbutifolia



Prunus ilicifolia ssp. *ilicifolia*



Rhus integrifolia



Sambucus mexicana

Latin name	Common name	Native to...	Form	height (ft)	spread (ft)	light level			moisture				uses					
						full sun	part shade	full shade	low	moderate	regular	moist	parks	promenade	revetment or slope	streetscape	bioswale	screening
Small trees and tree-like shrubs																		
<i>Arctostaphylos catalinae</i>	Catalina Manzanita	Channel Isl.	broadleaf evergr. tree/shrub	18	12	*	*		*				*	*				
<i>Ceanothus arboreus</i>	Island Ceanothus	Channel Isl.	broadleaf evergr. tree/shrub	15-20	12	*			*	*			*					*
<i>Comarostaphylis diversifolia</i>	Summer Holly	California	broadleaf evergr. tree/shrub	6-10	8	*	*	*		*			*	*				*
<i>Heteromeles arbutifolia</i> var. <i>macrocarpa</i>	Island Toyon	Channel Isl.	broadleaf evergr. tree/shrub	15	15	*	*		*	*			*					*
<i>Prunus ilicifolia</i> ssp. <i>ilicifolia</i>	Hollyleaf Cherry	California	broadleaf evergr. tree/shrub	3-20	10+	*	*		*				*					*
<i>Sambucus mexicana</i>	Mexican Elderberry	California	decid shrub/tree	6-20	15+	*	*					*					*	*
<i>Xylococcus bicolor</i>	Mission Manzanita	California	broadleaf evergr. tree/shrub	8	6	*	*		*	*			*	*				
Palm trees																		
<i>Brahea armata</i>	Blue Hesper Palm	Baja Calif.	palm tree	45	10	*			*	*			*	*	*			
<i>Brahea edulis</i>	Guadalupe Palm	Baja Calif.	palm tree	15	10	*	*		*	*			*	*	*			
Large shrubs																		
<i>Arctostaphylos glandulosa</i> ssp. <i>crassifolia</i>	Del Mar Manzanita	California	evergreen shrub	6-15	8-10	*	*		*				*	*				
<i>Calycanthus occidentalis</i>	Spice Bush	California	decid shrub	6-12	8	*	*			*	*						*	*
<i>Ceanothus griseus</i> 'Santa Ana'	Santa Ana Ceanothus	California	evergreen shrub	6	12	*	*			*	*						*	
<i>Ceanothus impressus</i>	Santa Barbara Ceanothus	California	evergreen shrub	5-10	8	*	*		*				*	*				
<i>Dendromecon harfordii</i>	Channel Island Bush Poppy	Channel Isl.	evergreen shrub	6-15	12	*			*				*	*				*
<i>Lavatera assurgentiflora</i>	Island Tree Mallow	California	evergreen shrub	10-15	10	*			*	*			*	*				*
<i>Rhus integrifolia</i>	Lemonadeberry	California	evergreen shrub	5-15	10+	*	*		*				*		*		*	*

recommendations: landscape

Plant Palette

- Medium shrubs



Arctostaphylos insularis



Ceanothus 'Joyce Coulter'



Ceanothus 'Wheeler Canyon'



Eriogonum arborescens



Eriogonum giganteum



Keckiella cordifolia



Malacothamnus clementinus



Ribes malvaceum 'Dancing Tassels'



Ribes thacherianum



Ribes viburnifolium



Rosa californica



Salvia leucophylla 'Point Sal'

Latin name	Common name	Native to...	Form	height (ft)	spread (ft)	light level			moisture				uses					
						full sun	part shade	full shade	low	moderate	regular	moist	parks	promenade	revetment or slope	streetscape	bioswale	screening
Medium shrubs																		
<i>Amorpha fruticosa</i>	False Indigobush	California	decid shrub	3-8	8	*	*	*			*	*					*	
<i>Arctostaphylos insularis</i> 'Canyon Sparkles'	Canyon Sparkles Manzanita	Channel Isl.	evergreen shrub	4	6		*	*		*			*	*		*		
<i>Ceanothus</i> 'Joyce Coulter'	Joyce Coulter Ceanothus	California	evergreen shrub	3-5	12	*			*	*			*	*		*		
<i>Ceanothus ramulosus</i> var. <i>fasciculatus</i>	Coast Ceanothus	California	evergreen shrub	5-8	6+	*			*				*	*				
<i>Ceanothus</i> 'Wheeler Canyon'	Wheeler Canyon Ceanothus	California	evergreen shrub	5-6	6	*			*	*							*	
<i>Eriogonum arborescens</i>	Santa Cruz Island Buckwheat	Channel Isl.	evergreen shrub	2-5	5	*	*		*	*			*	*		*		*
<i>Eriogonum cinereum</i>	Ashy Leaf Buckwheat	California	evergreen shrub	2-5	6	*			*				*	*		*		
<i>Eriogonum giganteum</i>	St. Catherine's Lace	Channel Isl.	evergreen shrub	3-6+	6	*			*	*			*					*
<i>Keckiella cordifolia</i>	Heart-Leaved Penstemon	California	semi-evergr shrub/vine	3+	5+	*	*	*		*	*						*	
<i>Lupinus chamissonis</i>	Dune Lupine	California	evergreen shrub	3-5	4	*	*		*	*			*	*	*	*		
<i>Malacothamnus clementinus</i>	San Clemente Bush Mallow	Channel Isl.	evergreen shrub	4-5	8+	*	*		*				*	*				
<i>Ribes malvaceum</i> 'Dancing Tassels'	Dancing Tassels Currant	Channel Isl.	deciduous shrub	6	5	*	*	*	*	*			*					
<i>Ribes thacherianum</i>	Santa Cruz Island Gooseberry	Channel Isl.	deciduous shrub	3-8	6		*	*		*	*		*				*	
<i>Ribes viburnifolium</i>	Catalina Perfume	Channel Isl.	evergreen shrub	3-4	6	*	*	*	*	*			*	*		*	*	
<i>Rosa californica</i>	California Wild Rose	California	semi-deciduous shrub	3-6	5+	*	*	*	*	*			*	*		*	*	
<i>Salvia leucophylla</i> 'Point Sal'	Point Sal Purple Sage	California	semi-evergreen shrub	3-4	4-6	*			*	*			*	*		*	*	

recommendations: landscape

Plant Palette

- Small shrubs



Artemisia nesiotica



Artemisia palmeri



Coreopsis gigantea



Encelia californica



Eriogonum grande var. *rubescens*



Eriogonum parvifolium



Eriophyllum nevinii



Galvezia speciosa



Iva hayesiana



Lepichinia fragrans



Salvia brandegei



Venegasia carpesioides

Latin name	Common name	Native to...	Form	height (ft)	spread (ft)	light level			moisture				uses						
						full sun	part shade	full shade	low	moderate	regular	moist	parks	promenade	revetment or slope	streetscape	bioswale	screening	
Small shrubs																			
<i>Artemisia nesiotica</i>	Island Sagebrush	Channel Isl.	evergreen shrub	1-2	2-4	*	*		*	*			*	*	*				
<i>Artemisia palmeri</i>	San Diego Sagewort	California	woody perennial	2-3	3	*	*				*						*		
<i>Coreopsis gigantea</i>	Giant Coreopsis	California	semi-evergreen shrub	3-4	2	*	*		*				*						
<i>Encelia californica</i>	Coast Sunflower	California	evergreen subshrub	3	4	*	*		*				*	*	*	*			
<i>Eriogonum grande var. rubescens</i>	Red Buckwheat	California	evergreen subshrub	2	6	*	*		*	*			*	*	*				
<i>Eriogonum parvifolium</i>	Coastal Buckwheat	California	evergreen subshrub	1-2	4+	*	*		*				*	*	*				
<i>Eriophyllum nevini</i>	Catalina Silverlace	Channel Isl.	evergreen subshrub	3	4	*	*		*	*			*						
<i>Galvezia speciosa</i>	Island Bush Snapdragon	Channel Isl.	evergreen shrub	3	5	*	*			*			*	*	*	*		*	
<i>Isocoma menziesii var. menziesii</i>	Menzies' Goldenbush	California	evergreen subshrub	1-3	3	*	*		*				*	*					
<i>Iva hayesiana</i>	Poverty Weed	California	evergreen shrub	1-3	5	*	*			*							*	*	
<i>Lepechinia fragrans</i>	Fragrant Pitcher Sage	Channel Isl.	semi-evergreen shrub	3	4	*	*		*	*			*	*		*			
<i>Salvia brandegei</i>	Brandegee's Sage	California	semi-evergreen shrub	3-4	4	*	*		*				*	*		*			
<i>Venegasia carpesioides</i>	Canyon Sunflower	California	semi-evergreen subshrub	3-5	3	*	*	*		*	*	*	*				*		

recommendations: landscape

Plant Palette

- Groundcovers
- Grasses and grass-like plants



Arctostaphylos uva-ursi



Arctostaphylos 'Pacific Mist'



Artemisia californica 'Canyon Gray'



Baccharis pilularis 'Pigeon Point'



Carex pansa



Ceanothus gloriosus 'Point Reyes'



Ceanothus griseus horizontalis
'Yankee Point'



Ceanothus maritimus



Juncus patens



Leymus condensatus 'Canyon
Prince'



Muhlenbergia rigens



Nasella pulchra

Latin name	Common name	Native to...	Form	height (ft)	spread (ft)	light level			moisture				uses					
						full sun	part shade	full shade	low	moderate	regular	moist	parks	promenade	revetment or slope	streetscape	bioswale	screening
Groundcovers																		
<i>Adenostoma fasciculatum</i> 'Nicolas'	Prostrate Chamise	Channel Isl.	groundcover	1	3-5	*	*		*				*	*	*			
<i>Arctostaphylos edmundsii</i> 'Carmel Sur'	Carmel Sur Manzanita	California	groundcover	1-2	6	*	*			*			*	*	*			
<i>Arctostaphylos uva-ursi</i> 'Point Reyes'	Point Reyes Bearberry	California	groundcover	1	4-6	*	*	*	*				*	*	*			
<i>Arctostaphylos</i> 'Pacific Mist'	Pacific Mist Manzanita	California	groundcover	2	6	*	*		*	*			*	*	*	*		
<i>Artemisia californica</i> 'Canyon Gray'	Canyon Gray Sagebrush	Channel Isl.	groundcover	1-2	6	*	*		*				*	*	*			
<i>Baccharis pilularis</i> 'Pigeon Point'	Dwarf Coyote Bush	California	groundcover	1	5	*	*		*	*			*	*	*		*	
<i>Ceanothus gloriosus</i> 'Point Reyes'	Point Reyes Ceanothus	California	groundcover	1	4-6	*	*		*	*			*	*	*			
<i>Ceanothus griseus horizontalis</i> 'Yankee Point'	Carmel Creeper	California	groundcover	2-3	6+	*	*		*	*			*	*	*	*	*	
<i>Ceanothus maritimus</i>	Maritime Ceanothus	California	evergreen shrub	1-3	6	*	*		*	*			*	*	*			
<i>Eriogonum fasciculatum</i> 'Dana Point'	Dana Point Buckwheat	California	evergreen shrub	1-3	5+	*	*		*				*	*	*			
Grasses and grass-like plants																		
<i>Aristida purpurea</i>	Purple Three-Awn	California	bunchgrass	1-2	1	*	*		*				*	*		*	*	
<i>Carex pansa</i>	California Meadow Sedge	California	creeping perennial	<1	2+	*	*	*		*	*		*			*	*	
<i>Carex praegracilis</i> **	California Field Sedge	California	creeping perennial	1	2+	*	*	*		*	*		*	*			*	
<i>Deschampsia caespitosa</i> **	Tufted Hairgrass	California	perennial bunchgrass	3	2	*	*	*	*	*	*		*	*			*	
<i>Distichlis spicata</i>	Salt Grass	California	creeping perennial	<1	3+	*	*			*	*	*	*			*	*	
<i>Festuca rubra molate</i> **	Molate Red Fescue	California	perennial grass	1-3	2	*	*		*	*			*	*				
<i>Juncus patens</i>	California Gray Rush	California	perennial rush	2	2+	*	*	*	*	*	*	*					*	
<i>Leymus condensatus</i> 'Canyon Prince'	Canyon Prince Wild Rye	California	bunchgrass	3-4	3	*	*		*	*			*	*		*	*	
<i>Leymus triticoides</i> 'Grey Dawn' **	Grey Dawn' Creeping Wild Rye	California	perennial grass	2	3+	*	*	*		*	*	*					*	
<i>Muhlenbergia rigens</i>	Deergrass	California	bunchgrass	2	2	*	*		*				*	*		*	*	
<i>Nasella pulchra</i>	Purple Needlegrass	California	bunchgrass	2	2	*	*		*	*			*	*		*		
<i>Sporobolus airoides</i>	Alkali dropseed	California	perennial bunchgrass	2	3	*			*	*	*						*	

** Can be used in turf applications, including turf blends. Commercial turf blends should be slow growing and drought tolerant to minimize maintenance costs.

recommendations: landscape

Plant Palette

- Succulents
- Vines
- Perennials



Artemisia douglasiana



Calystegia macrostegia



Camissonia cheiranthifolia



Castilleja affinis



Coreopsis maritima



Dudleya hassei



Dudleya virens



Epilobium 'Catalina'



Heuchera maxima



Iris douglasiana



Limonium californicum



Mimulus cardinalis

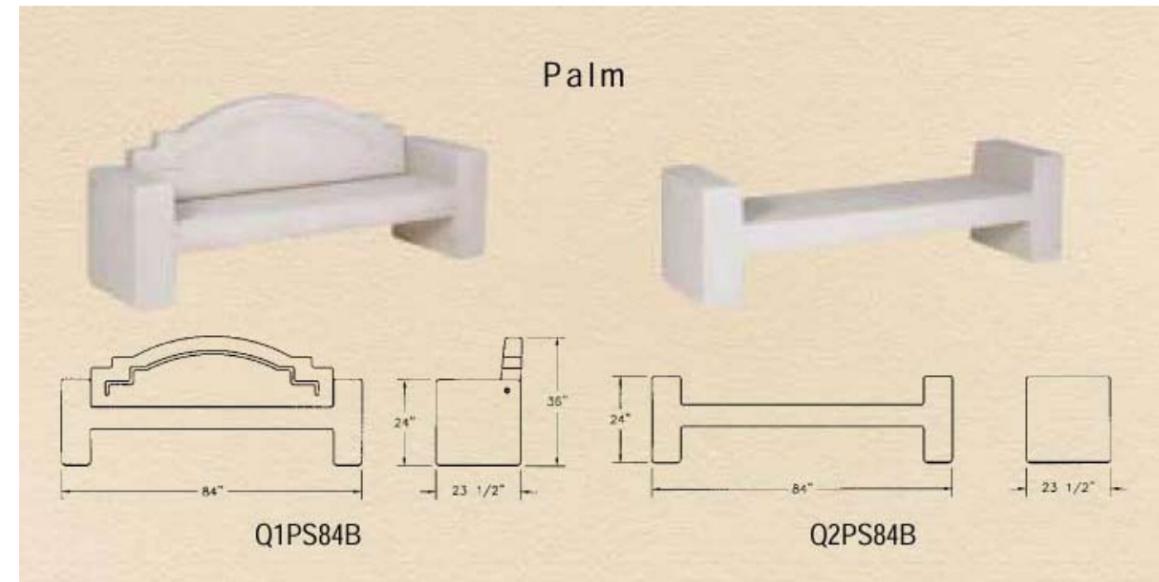
Latin name	Common name	Native to...	Form	height (ft)	spread (ft)	light level			moisture				uses					
						full sun	part shade	full shade	low	moderate	regular	moist	parks	promenade	revetment or slope	streetscape	bioswale	screening
Succulents																		
<i>Dudleya hassei</i>	Catalina Live-forever	Channel Isl.	succulent	1	1	*	*	*	*				*					
<i>Dudleya virens</i>	Green Live-forever	California	succulent	1	1	*	*	*	*				*					
Vines																		
<i>Calystegia macrostegia</i> 'Anacapa Pink'	Island Morning-glory	Channel Isl.	evergreen vine	climber		*	*			*			*	*		*		*
Perennials																		
<i>Artemisia douglasiana</i>	Mugwort	California	herb. perennial	2-3	3	*	*	*			*							*
<i>Camissonia cheiranthifolia</i>	Beach Evening Primrose	California	herb. perennial	1	3	*	*		*				*					
<i>Castilleja affinis</i>	Red Paintbrush	California	herb. perennial	1-2	1-2	*	*		*	*			*	*				
<i>Coreopsis maritima</i>	Sea Dahlia	California	herb. perennial	2	3	*			*				*					
<i>Dichondra occidentalis</i>	Western Dichondra	California	herb. perennial	<1	2		*	*	*	*			*					
<i>Epilobium 'Catalina'</i>	Catalina Fuchsia	Channel Isl.	herb. perennial	1-3	4	*	*		*				*					
<i>Huechera maxima</i>	Island Alum Root	California	evergreen perennial	1	1-2		*	*	*	*			*					*
<i>Iris douglasiana</i>	Douglas Iris	California	herb. perennial	1	2+	*	*	*	*	*								*
<i>Limonium californicum</i>	Coastal Statice	California	herb. perennial	1	2+	*	*				*	*	*					*
<i>Mimulus cardinalis</i>	Scarlet Monkeyflower	California	herb. perennial	1-4	4	*	*	*			*	*						*
<i>Polypodium californicum</i>	California Polypody	California	summer-dormant fern	1	1		*	*			*	*						*

recommendations: landscape

Furniture: Seating

- Compatible with donor program
- Durable and affordable
- Flexible seating options

Freestanding benches, with and without backs, are to be used along the promenade and in parks. The recommended bench is based on the existing concrete donor bench located throughout the Harbor. Input from public workshops and Harbor staff indicate that this is a preferred bench type. These precast concrete benches are solid, durable, and affordable. Not only can they incorporate donor plaques, but they can have customized finish treatments (e.g., sandblasting) if desired. The backless benches should be used in areas to preserve views or where people need the option to sit facing different directions. Benches should be placed on concrete pads adjacent to public walkways.



Precast concrete bench, available through Quik Crete. Standard colors and finishes are more affordable. Suggested colors from this manufacturer include Natural and Latte. Suggested finishes are Smooth and Light Sandblast (the least textured of the available standard finishes). Benches can accommodate a donor plaque program and be fitted with skateboard deterrents. Benches with and without backs are recommended.



Existing donor bench in Channel Islands Harbor.



Concrete benches are adjacent to the sidewalk at Channel View Park.



Example of a concrete seatwall that has been poured for a unique appearance.



Example of smooth concrete seatwalls, used in multiples here to create event seating.



Example of stone seatwalls on a slope.

Seatwalls are another flexible seating option with other benefits. They can be used as retaining walls to accommodate grade changes as well as seating. They also can vary in length and curvature, and can help guide pedestrian traffic and define outdoor spaces. Seatwalls are relatively inexpensive, and can be customized to incorporate different finishes, sandblasted lettering, lighting, and decorative elements. For Channel Islands Harbor, seatwalls are recommended in public parks and where retaining walls are needed adjacent to walkways.



Left: Precast concrete picnic tables like these are available through Wausau Tile (models shown: TF3130 and TF31287). Suggested colors include Misty Gray, Gray, Sand, or Brick Red. The option shown near left provides more convenient use for those in wheelchairs. Tables should be provided on concrete pads; those intended for wheelchair access should also be accessible from an ADA-compliant walkway.

Tables with integral seating also are recommended for Channel Islands Harbor. These include picnic tables and game tables. Picnic tables may be used in the Harbor's parks (including pocket parks). At least one ADA-accessible table should be available in each park. Game tables can be integrated in parks and adjacent to the waterside promenade.



Left: The precast concrete game tables shown here are available through Wausau Tile (models shown: TF3175 and TF3180). Suggested colors include Misty Gray, Gray, Sand, or Brick Red. Tables should be provided on concrete pads. The two-seat option (far left) is especially suitable for placement along the waterside promenade.

recommendations: landscape

Furniture

- Drinking fountains
- Trash receptacles
- Bicycle racks

Drinking fountains should be provided in the Harbor's parks and, if possible, with its public restrooms. The family of drinking fountains recommended includes ADA-accessible fountains and a style with a separate bubbler for dogs. Options are available in concrete and metal. The concrete fountains pictured at right are by Peterson; the recommended finish is Lite Sand Blasted texture in Dove Gray or Sand Tan. The metal fountains shown below are manufactured by Kay Park, and are available in powder-coated steel. Recommended finish colors are green (shown in images), blue, or gray.



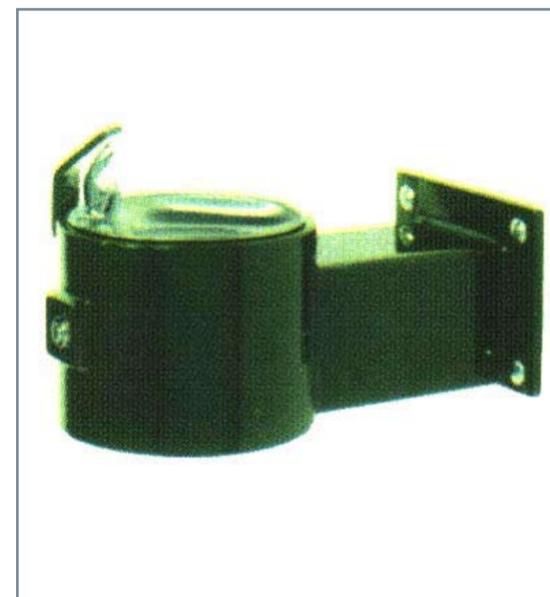
Concrete wall-mounted exterior drinking fountain for use with restrooms and other buildings.



Concrete ADA-accessible drinking fountain.



Steel ADA-accessible drinking fountain.



Steel wall-mounted exterior drinking fountain for use with restrooms and other buildings.

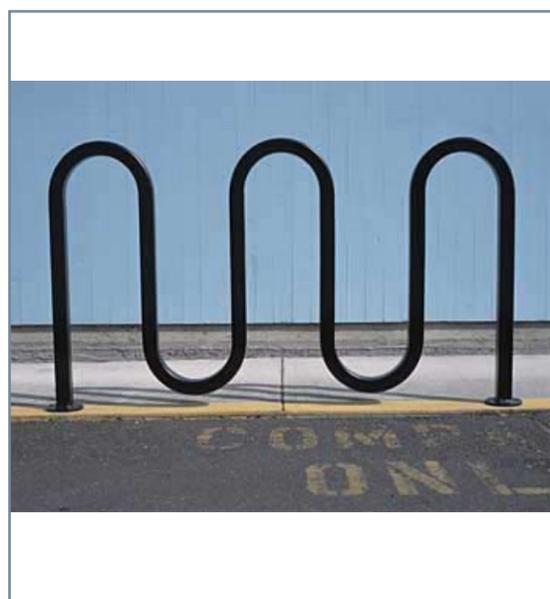


Steel drinking fountain with separate bubbler for dogs.



Options for precast concrete trash receptacles (left and center, from Wausau Tile; right, from Quick Crete). Recommended finish for Wausau Tile trash receptacles is Weatherstone (Gray, Sand, or Buff). Quick Crete receptacle requires a custom graphics template for sandblasted design.

Trash receptacles should be associated with benches, tables, interpretive areas, and informational kiosks. Three options are shown. The option with the two-tone graphics is a custom product; the other options may be less costly or complicated to procure. If desired, some receptacles can be identified for use as recycling containers by specifying cast-in-place or silk-screened logos. Trash receptacles should be installed to be resistant to rats and birds, and should be easily serviced and maintained. Enclosures should be provided for common or multiple trash receptacles and recycling containers. Trash and recycling enclosures should also provide information on designated disposal/recycling locations for batteries and oil, for use by boaters.



Options for bicycle racks (left, from Huntco; right two, from Landscape Forms). Huntco line is available in many sizes to accommodate different numbers of bicycles. Options are available in stainless steel. Huntco also available in steel with powder coat finish or thermoplastic coating (Hunter Green, Royal Blue, or Light Gray recommended).

Bicycle racks should be distributed among the Harbor's parks and business destinations. According to the City of Oxnard Bicycle and Pedestrian Facilities Master Plan (September 24, 2002), 50 decentralized spaces are suggested for Channel Islands Harbor. Because of its durable finish, stainless steel is recommended for the bicycle racks, although other color options are available to add interest in public areas.

recommendations: signage

Objectives for Signage

- **Improve signage and way finding elements to make the Harbor more visitor-friendly and market the Harbor's businesses and events**
- **System that is fun and engaging, and brings excitement and interest to the Channel Islands Harbor**
- **Facilitate way finding and circulation in the Harbor from a vehicular & pedestrian standpoint**
- **Develop consistent signage that unifies the Harbor and promotes its identity**
- **Highlight points of interest with informational and identity signage**
- **Consider signs to educate visitors about the area's cultural and natural resources**
- **Introduce banners and signs to promote local events**

Design Criteria

- Develop a "kit of parts" for all sign types including Gateway, Direction, Information and Regulation signs
- Use signs that are scaled appropriately to site context
- Consistent use of color, image, typography
- Use standard DOT pictograms for communication over words when possible
- Limit messages on signs to 3-5 lines of messages
- Comply with ADA codes as required
- Maintain adequate contrast between background color/material and type/image for maximum legibility
- Incorporate lighting (internal illumination or ground lighting) for night time visibility and security
- Maintain consistency of sign locations within the Harbor
- Develop educational and informative exhibits that integrate with the furniture program and communicate the stories of the area. Possible subjects are: Channel Islands Harbor History, Natural Habitats, Indigenous Flora, Island Wildlife and Marine Species, Whale Watching, Shipwrecks in the Channel Islands, etc.

Sign Location Plan

Figure 3.4-1 presents a sign location plan. The purpose of the sign location plan is to recommend proposed locations for signs. The signs are organized in 4 categories which are distinguished by shape & color based on their function.

- Identity signs identify specific places and are used to welcome people to the project and identify locations and services. Identity elements should reflect the specific project's brand identity (corporate logo if one exists) and be clearly visible.
- Direction signs are used help to direct people to destinations within the project.
- Information signs offer detailed information about a place so that one can quickly identify and locate their target destination - an example of this sign type is a directory.
- The amenities category increases the interest and attractiveness of projects. Amenities can be considered environmental art that add to the personality of the project, or elements that inform, entertain and/or educate the visitor.

Signage Concepts

Figure 3.4-2 shows existing Harbor identity graphic elements. From these and other information, recommended fonts and color and icon palettes are presented in Figures 3.4-3 and 3.4-4. A series of inspirational images and interpretive opportunities, from which signage concepts also have been derived, are shown in Figures 3.4-5 through 3.4-9. Our recommended concepts for different signage types for Channel Islands Harbor are depicted in Figures 3.4-10 through 3.4-20.

Sign Location Plan

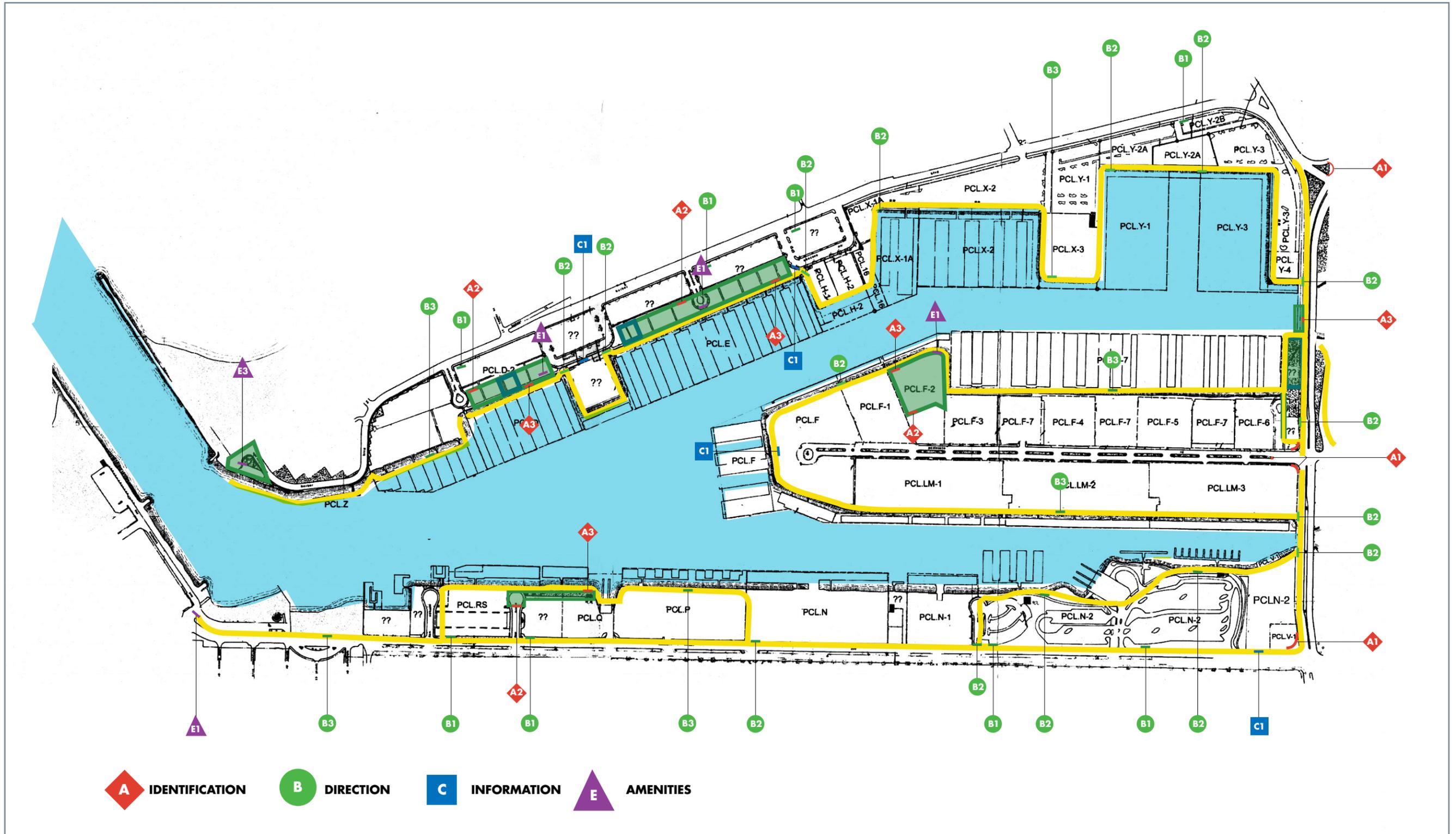


Figure 3.4-1

Current Identity

1 COLOR PALETTE



2 PRIMARY IDENTITY



3 SECONDARY IDENTITY

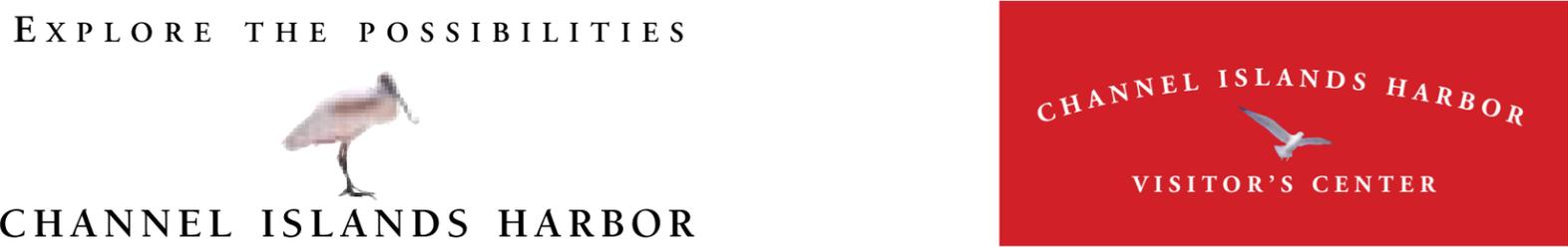


Figure 3.4-2

Colors, Materials and Fonts

1 COLOR PALETTE

PMS 2757C PMS 653C PMS 7456C PMS 131C PMS 173C PMS 187C

2 MATERIAL PALETTE

Stainless Steel Painted Metal Matthew Paint MP06175 Terrazzo with Mother of Pearl

3 MRS EAVES - SMALL CAPS

A B C D E F G H I J K L M N O P R S T U V W X Y Z
 1 2 3 4 5 6 7 8 9 0

4 FUTURA

A B C D E F G H I J K L M N O P R S T U V W X Y Z
 a b c d e f g h i j k l m n o p r s t u v w x y z
 1 2 3 4 5 6 7 8 9 0

Figure 3.4-3

Inspiration Images - Icons

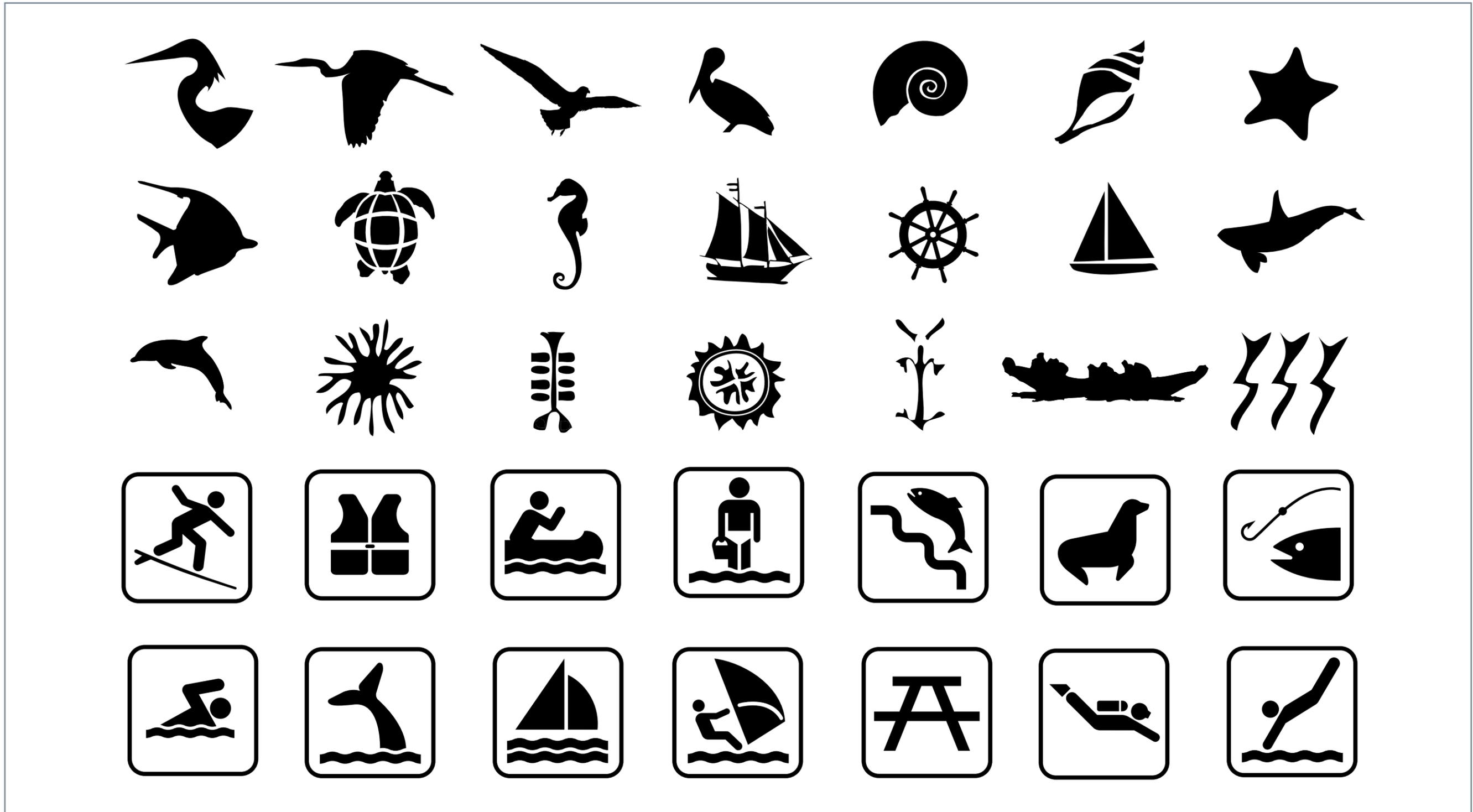


Figure 3.4-4

Inspiration Images - Sailing



Figure 3.4-5

Inspiration Images - Wildlife



Figure 3.4-6

Inspiration Images - Sea Life

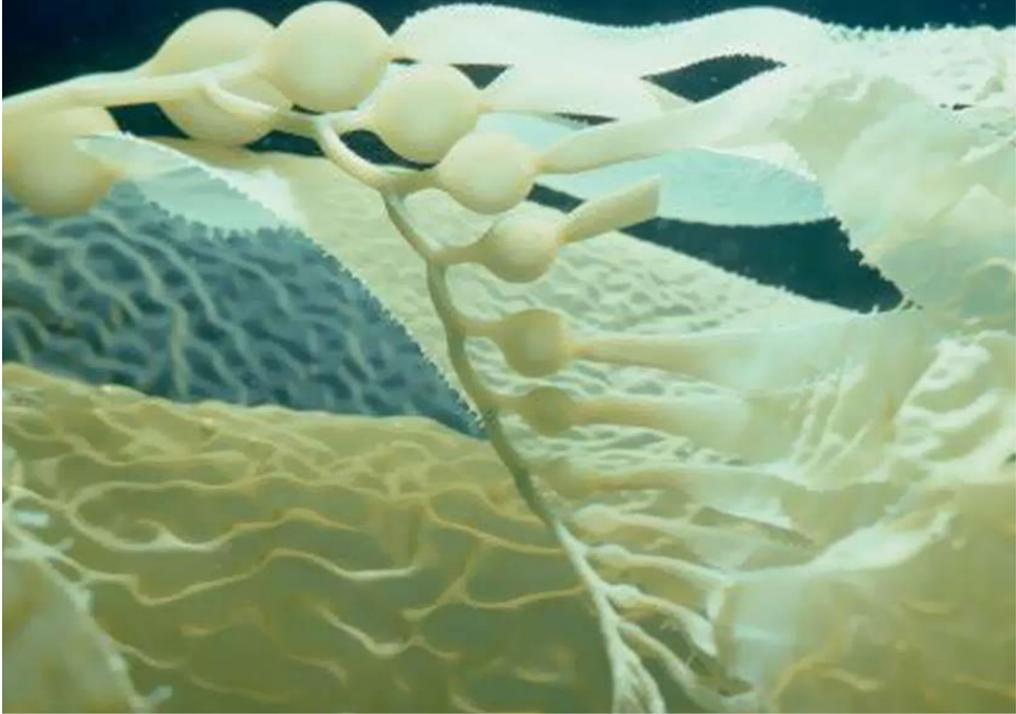
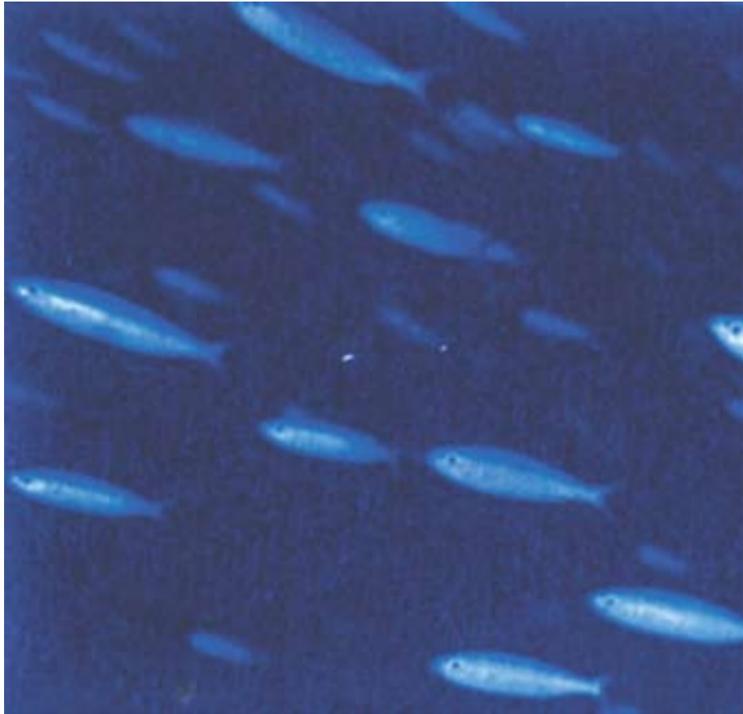


Figure 3.4-7

Inspiration Images - Texture

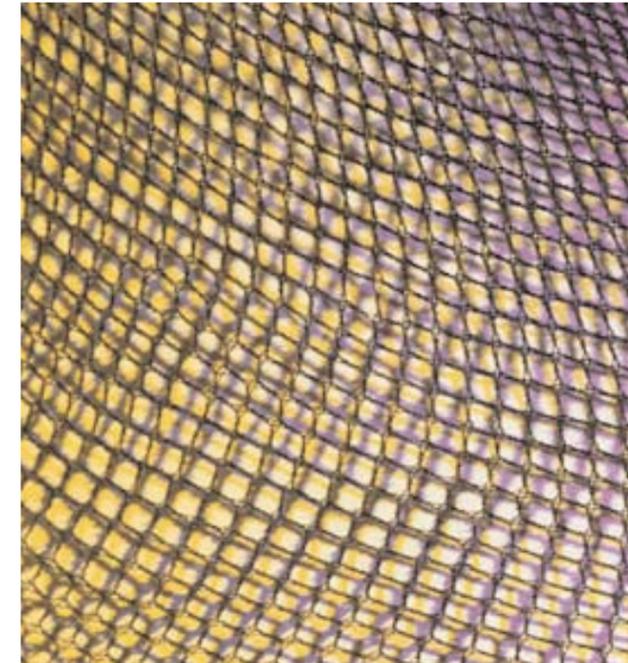
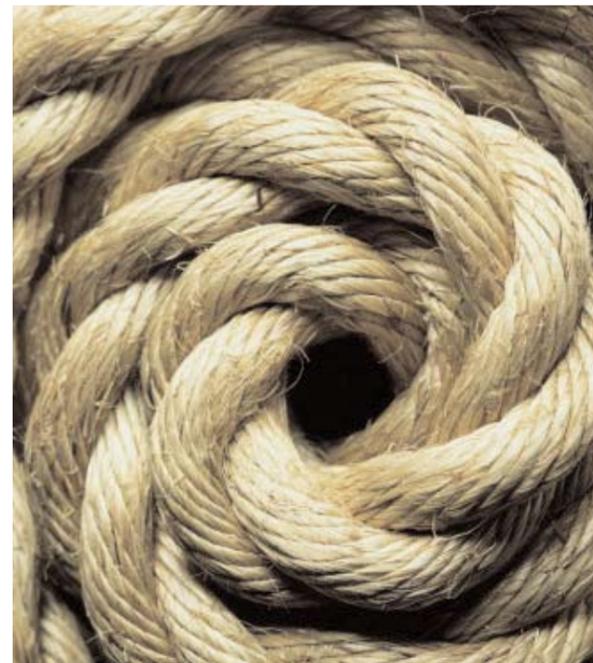
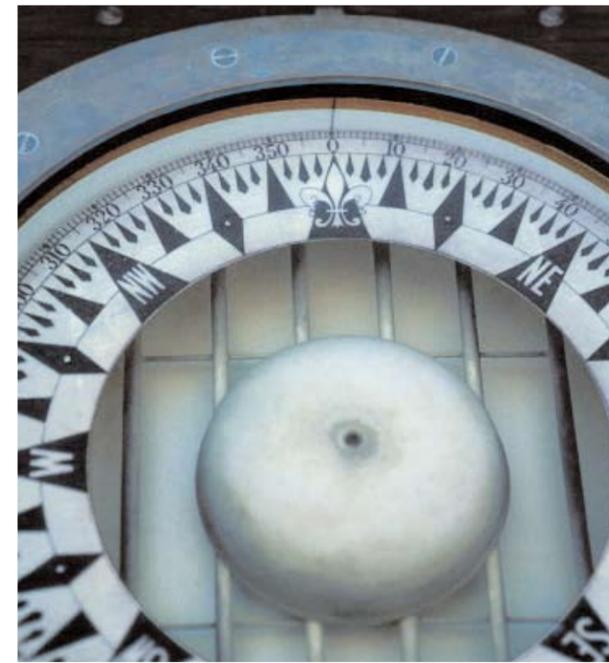
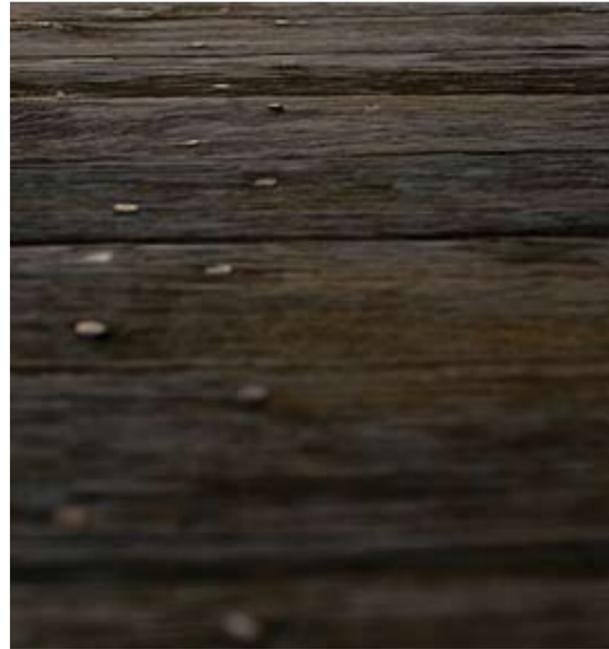
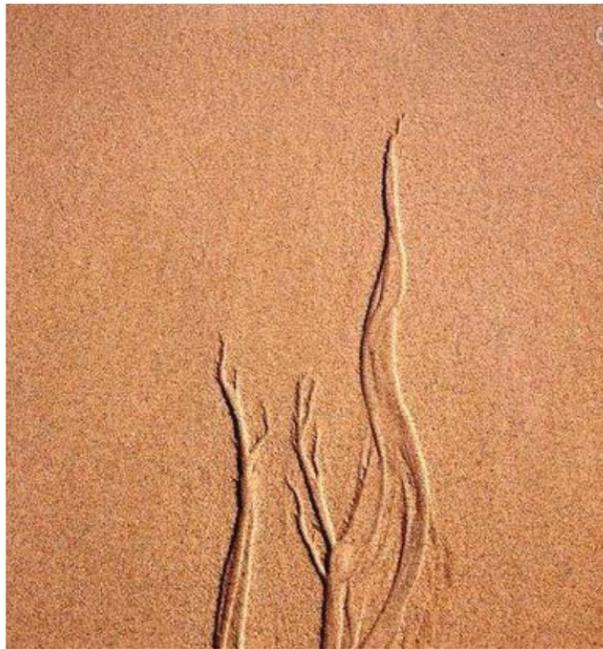


Figure 3.4-8

Story Telling Opportunities

Whales



1. From July to August of 2007 the Channel saw an increase of Blue Whales.
2. Many species of whales have been sighted around the Channel Islands
3. Whales are commonly seen in the Channel from June to October.
4. Sperm whales are the rarest whale to see in the Channel.
5. Humpback whales are a common visitor to the Channel.

Chumash Culture



1. Chumash Tomol Crossing.
2. The dance held in the highest regard is the Swordfish dance.
3. The Chumash lived mostly off the sea eating: fish, mussels and abalone.
4. The Chumash first settled in the Channel Islands area 13,000 years ago.
5. The Chumash had dances for the Swordfish, the Barracuda and the Seaweed.

Shipwrecks



1. Del Rio; Sunk in 1952 near Frenchy's cove because the ship caught on fire.
2. Lotus; Sunk in 1921 near Anacapa Island due to its cargo hold igniting.
3. Legend; Sunk in 1967 in Point Bennett from rocks ripping a hole in the keel.
4. Crown of England; Sunk in 1894 in Ford Point when stormy conditions caused it to be struck against the rocks.
5. Blue Fin J 245; Sunk in 1944, this Army supply ship was lost & its wreck location has never been accounted for.

Bird Nesting



1. Black Oyster Catcher is the Bird of December.
2. 11 out of 16 Southern California Sea birds breed in the Sanctuary.
3. 60 species of Sea birds use the Channel Island Sanctuary as a nesting ground.
4. Santa Barbara Islands are home of the largest Xantus Murrelet colony in the USA.
5. Anacapa Island is one of the few permanent breeding grounds for the endangered California Brown Pelican.

West Gateway



A1 Gateway Identity

The Gateway Identity consists of sail-like sculptural elements, channel letters and a water feature with fountains. The overall sculpture height is 30 feet minimum to 40 feet maximum. Sculptural elements are to be stainless steel posts varying in lengths from 25 to 35 feet high supporting fabricated painted aluminum “sails” which vary from 20 to 35 feet long by 6 to 15 feet wide. “Sails” are to be mounted in a static manner with minimal allowable support structures. Footings to be engineered for local wind loads.

“CHANNEL ISLANDS HARBOR” letters to be constructed of painted aluminum channel letters with silver backers mounted to concrete ledge in a tamper-proof manner. Letters to have internal white LED

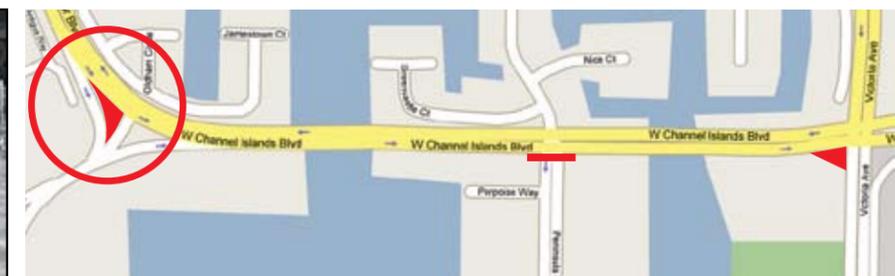


Figure 3.4-10

East Gateway



halo illumination with remote transformers. "WEST GATEWAY" letters to be flat cut-out painted ½" thick aluminum letters pin mounted to concrete, or 1" deep letters cast into concrete (debossed).

Sculpture elements, water feature and concrete ledge externally illuminated by in-ground uplights. Water feature to include fountain jets with control system.

Note: All signs and sculpture elements to be built and finished for marine climate.

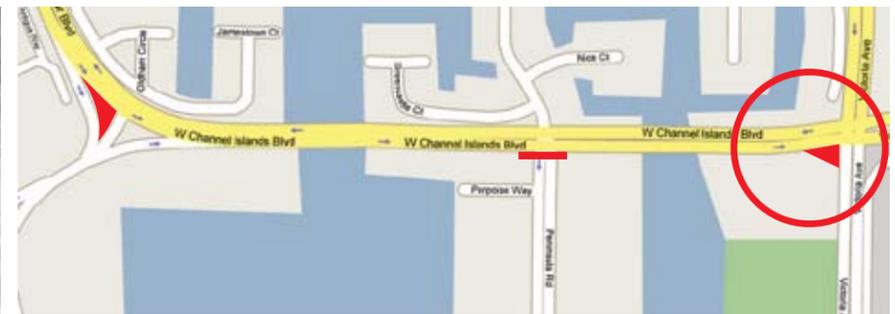


Figure 3.4-11

Peninsula Gateway - Option 1

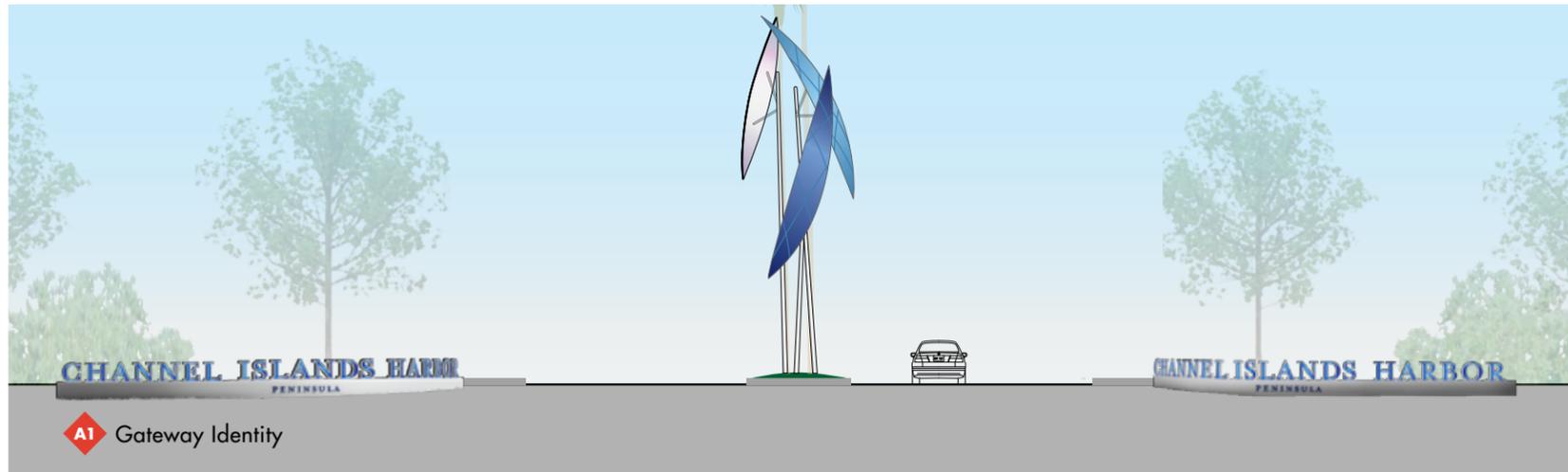


Figure 3.4-12

Peninsula Gateway - Option 2

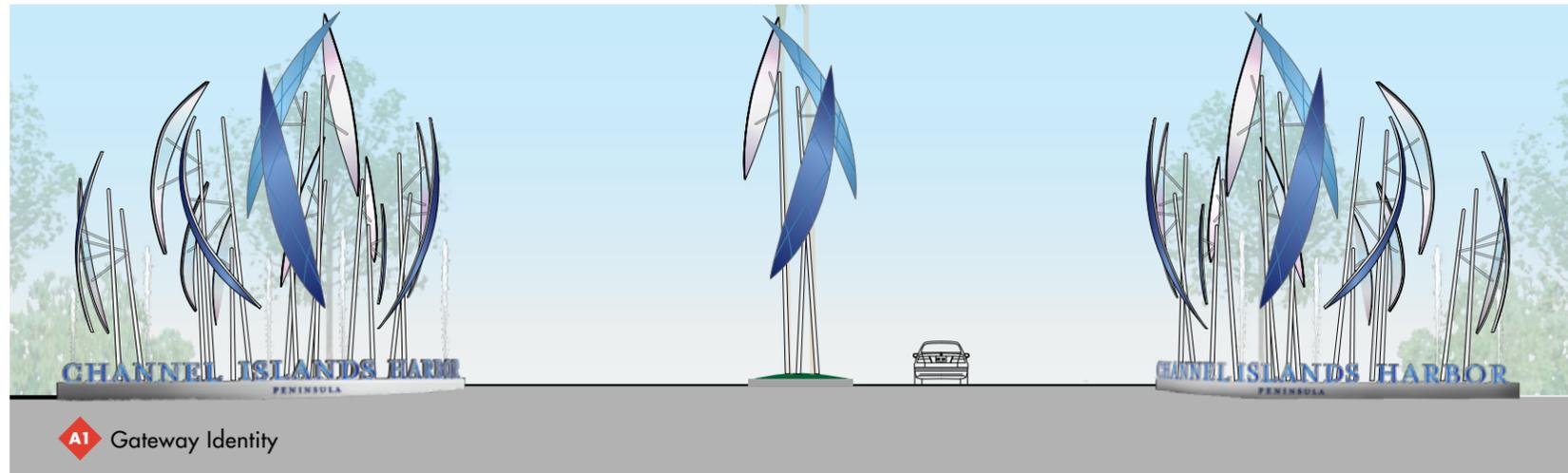
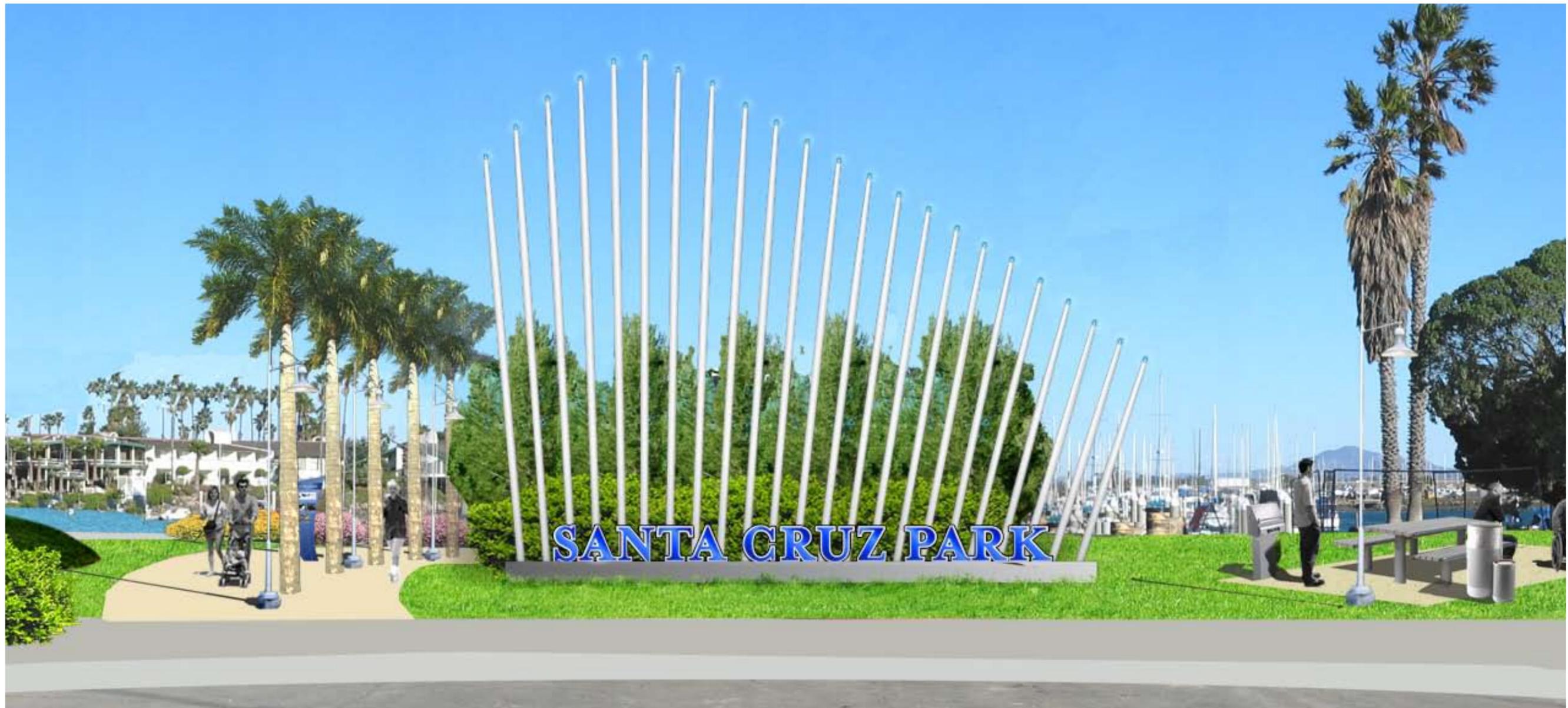


Figure 3.4-13



A2 Park Monument

Illuminated channel letters mounted to concrete base and tapered poles with LED illuminated tips. Painted metal tapered poles vary in length from 9 feet to 24 feet with LED illuminated tips. Provide footings as required. Park name to be constructed of painted aluminum channel letters with silver backer mounted to concrete ledge in a tamper-proof manner. Internal white LED halo illumination with remote transformers. Poles and concrete ledge externally illuminated by in-ground uplights.

A3 Park Marker

Painted tapered poles with LED illuminated tips and tubular sign panel sleeved over center tube. Painted metal tapered poles vary in length from 15 feet to 24 feet with LED illuminated tips. Provide footings as required. Park name to be applied vinyl letters on a painted aluminum sleeve. Poles externally illuminated by in-ground uplights.

Park Identity

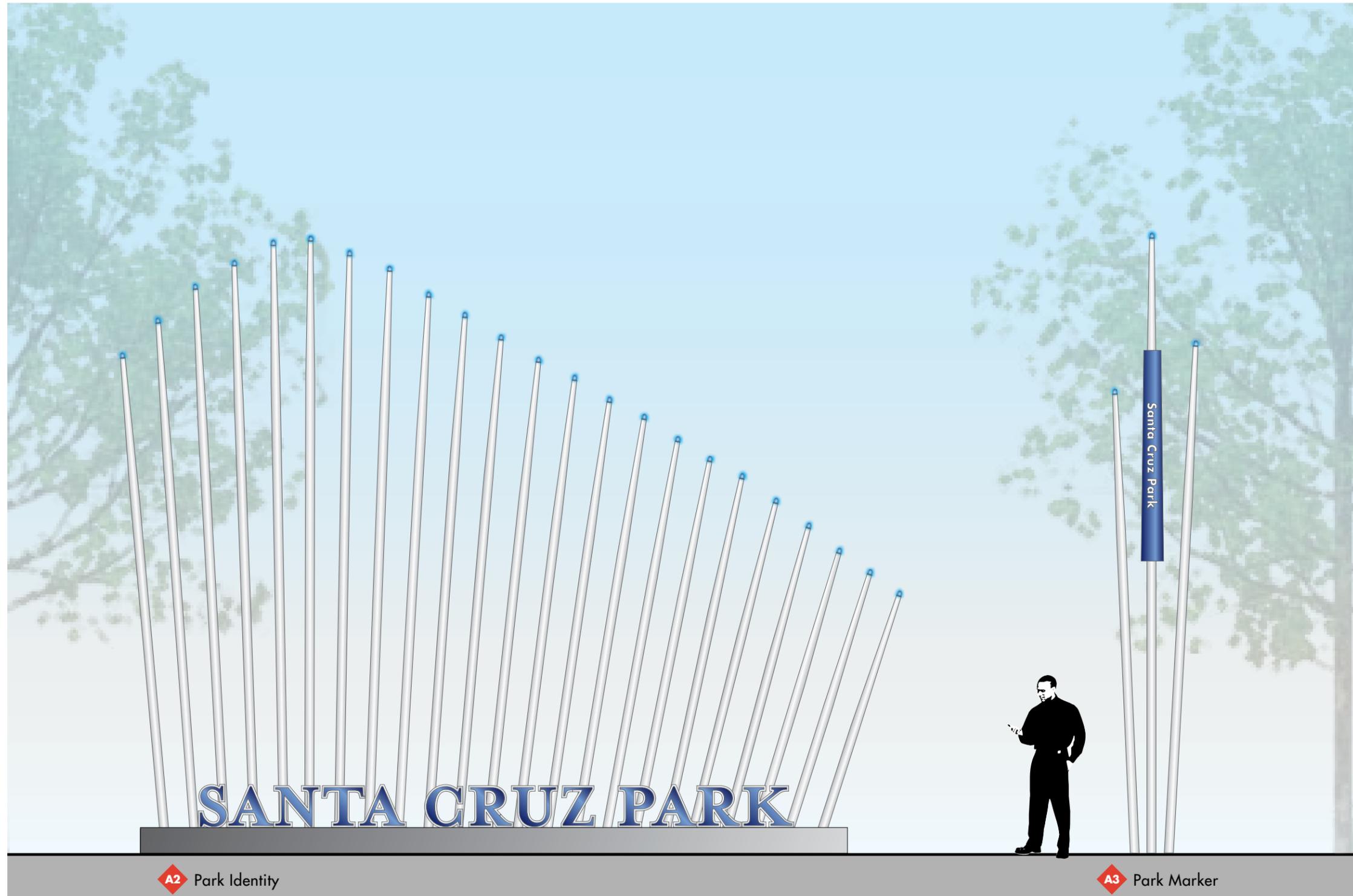
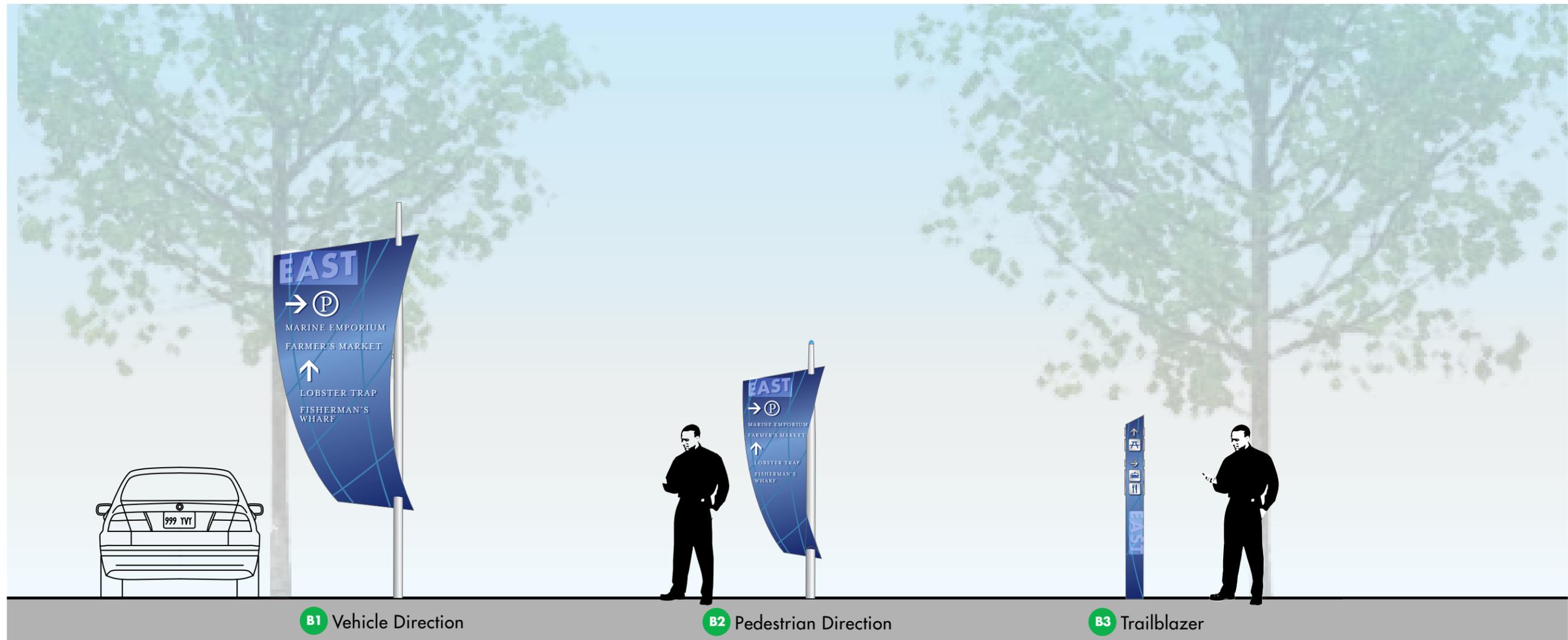


Figure 3.4-14

Direction & Information Signage



B1 Vehicle Direction

Single faced sign including sign panel and tapered post piercing. Painted and rolled aluminum sign panel with welded structure on back. Reflective white vinyl copy and symbols. Painted metal tapered pole approximately 16 feet tall. Provide footing as required. Sign externally illuminated by in-ground uplights.

B2 Pedestrian Direction

Single faced sign including sign panel and tapered post. LED illuminated post only for signs located on promenade. Painted & rolled aluminum sign panel with welded structure on back. Reflective white vinyl copy and symbols. Painted metal tapered pole approximately 10 feet tall. Provide footing as required. Sign externally illuminated by in-ground uplights.

B3 Trailblazer

Option 1: Painted square steel tube with welded angled top and pin-mounted letters and graphic symbols. Text "EAST" to be screened on steel tube. Option 2: Cast concrete with embossed and paint-filled graphics and symbols. Sign externally illuminated by in-ground uplights.

Figure 3.4-15

Pavement Graphics & Interpretive Exhibits



C1 Directory

Rolled aluminum panel and tapered post. LED illuminated post only for signs located on promenade. Painted & rolled aluminum panel with welded structure on back. Screened or printed vinyl graphics and images. Painted metal tapered pole approximately 10 feet tall. Provide footing as required. Sign externally illuminated by in-ground uplights.

E1 Interpretive Exhibit

Painted & rolled aluminum panel with welded structure on back. Screened or printed vinyl graphics and images. Provide footing as required. Sign illuminated by in-ground uplight.

Figure 3.4-16

Banners

Banners are a great and easy way to feature the Harbor's rich history and wildlife as well as introduce color and promote local events.

E2 Banner

Double faced fabric banners mounted to light poles with banner mounting system. Establish a maintenance, cleaning and storage agreement with the banner fabricator to rotate banners on a regular basis based on an annual event program.

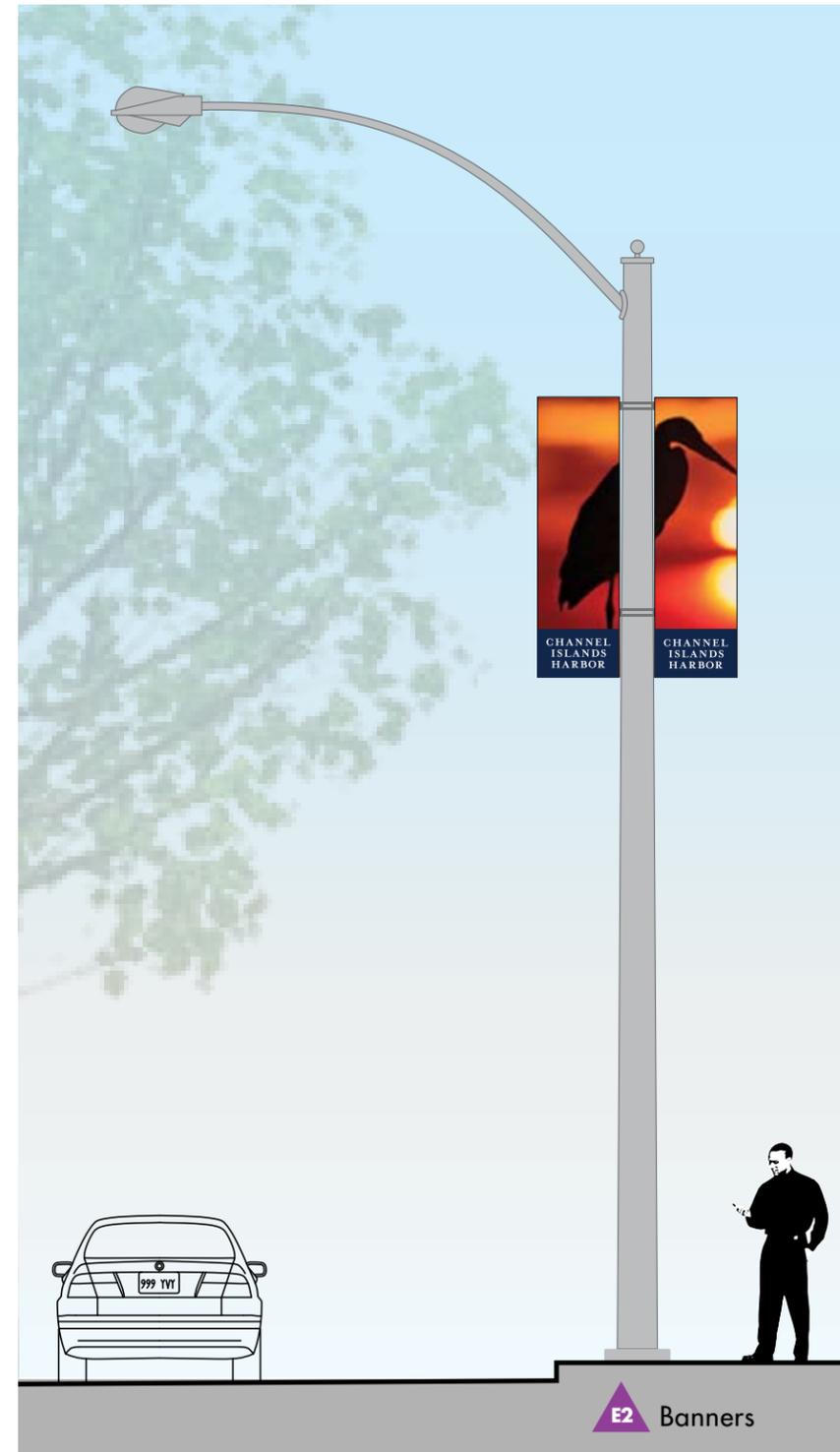


Figure 3.4-17

Banner System



Figure 3.4-18

Medallions



SANDBLASTED CONCRETE



TERRAZZO

E3 Medallions

Option 1: Sandblasted concrete with paint in fill.

Option 2: Cast or plate inlaid with Terrazzo, mounted to concrete with finished surface flush to finished concrete.

The examples shown above show park names, although the medallions can be designed to incorporate any content or imagery relevant to their immediate surroundings. Gathering areas and high pedestrian-traffic zones such as parks, plazas and entrances are great locations to feature medallions.

Figure 3.4-19

Sign Family



Figure 3.4-20

recommendations: tenant signage

Today, Channel Islands Harbor contains a wide range of signs and sign types for a variety of Lessees. The result is a cacophony of visual clutter which adds up to confused visitors, frustrated residents and traffic congestion. The goal of these tenant sign guidelines is to help Lessees identify their projects in a clear and consistent way in order to avoid “over signing” and eliminate visual clutter.

General Sign Design & Limits

- Projects within the Channel Islands Harbor should have their own visual identity yet incorporate some of the established Harbor’s character to add to the fabric of the Channel Islands Harbor experience.
- Sign size and quantity must be compatible with the scale of the leaseholder’s structure.
- Lessees are encouraged to bring a sculptural and dimensional quality to their signs. Letters and graphics incorporating depth can be especially useful in this regard.
- The dimensions and shape of sign panels or elements mounted on building facades or marquees shall be in architecturally acceptable proportion to the dimensions of the surface on which mounted.
- Double or multi-face signs shall count as one (1) unit when computing number of signs allowed.
- The area of one (1) face shall be used in computing the area of double or multi-faced signs.
- Sign elements shall project no greater than 2’-0” beyond the lease line unless reviewed and approved by the Harbor Commission and Board of Supervisors.
- Vendor and advertising kiosks should be incorporated into pedestrian-heavy areas to enhance the pedestrian experience within the project.

Commercial Sign Design Guidelines

Commercial project leaseholders are allotted a total of one (1) square foot of signage per linear foot of street frontage for building signs.

Commercial project leaseholders are encouraged to incorporate the following signs into their projects:

- Building Identity Sign
- Monument Sign (those which are detached from the building, freestanding and integrated into the landscape).

Retail Sign Design Guidelines

Retail project leaseholders are allotted a total of one (1) square foot of signage per linear foot of street frontage for building signs.

Retail project leaseholders are encouraged to incorporate the following signs into their projects:

- Retail Façade Identity Sign
- Retail Blade Identity Sign (requires 7'-6" clearance from grade)
- Secondary Wall-mounted identity sign (required to be mounted 60" from grade)

Hotel Sign Design Guidelines

Hotel project leaseholders are allotted a total of one (1) square foot of signage per linear foot of street frontage for building signs.

Hotel project leaseholders are encouraged to incorporate the following signs into their projects:

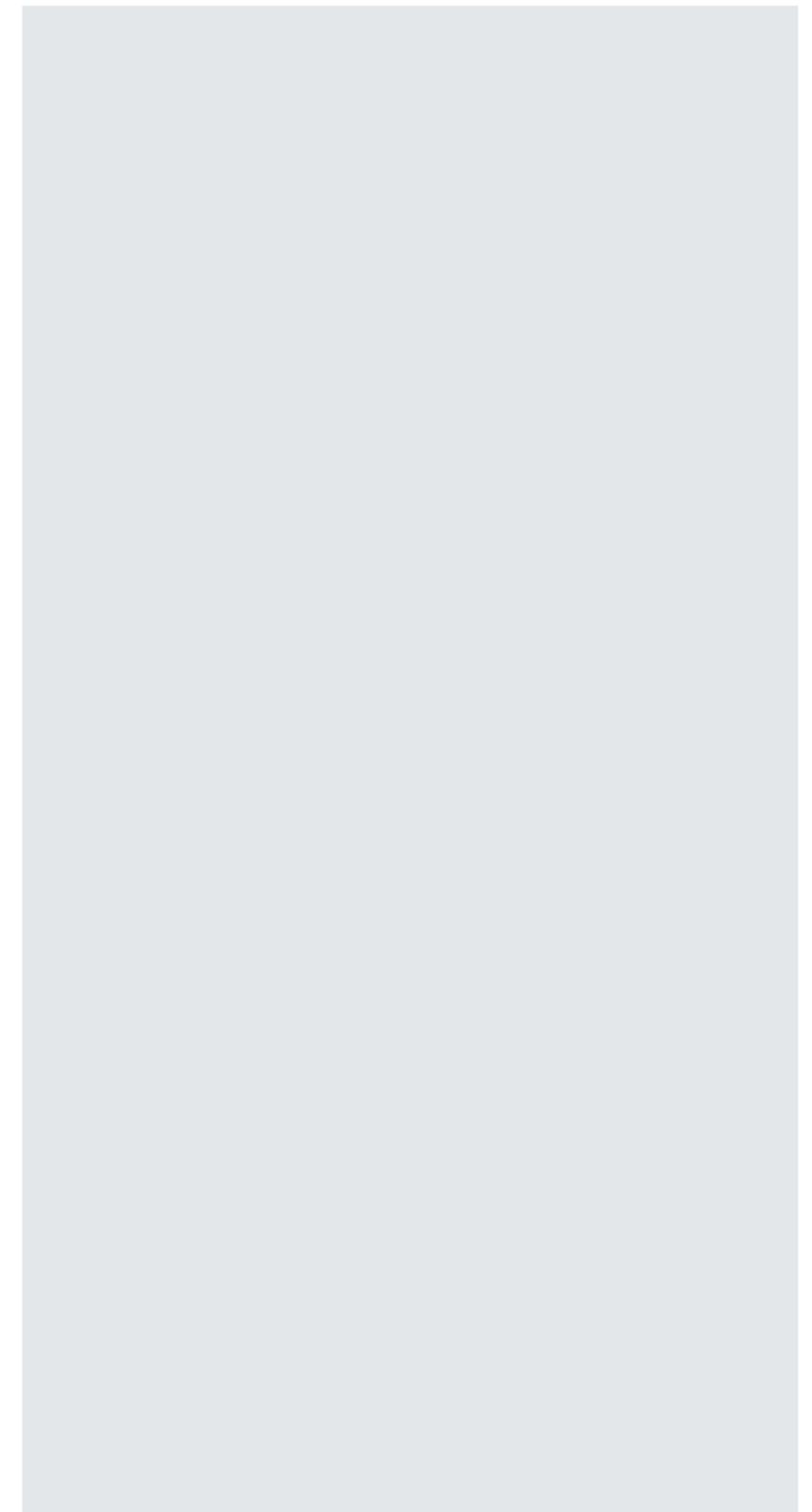
- Hotel Building Identity Sign
- Hotel Monument Sign
- Hotel Façade Identity Sign (leading into hotel lobbies)

Luxury Hotel & Private Club Sign Design Guidelines

Luxury hotel & private club project leaseholders are allotted a total of one (1) square foot of signage per linear foot of street frontage for building signs.

Luxury hotel & private club project leaseholders are encouraged to incorporate the following signs into their projects:

- Monument Sign
- Façade Identity Sign (leading into hotel and club lobbies)



recommendations: lighting

Lighting will:

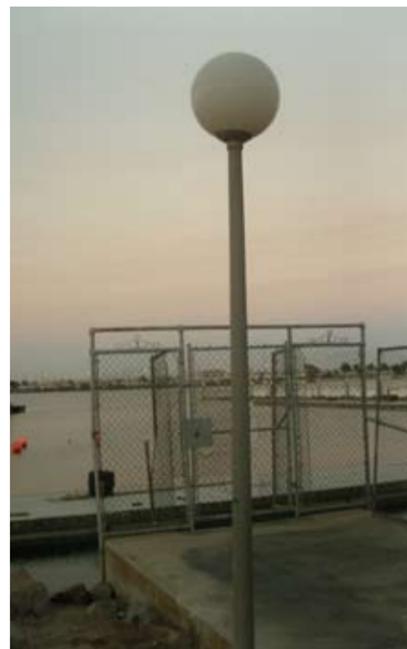
- **Maintain the quaint and residential feeling - currently a staple of the Harbor**
- **Unify the Harbor throughout with a memorable, distinctive ambiance**
- **Emphasize the natural architecture of the waterfront as well as the supportive elements of the landscape design**
- **Enhance feelings of safety and security**

General Observations

First and foremost, Channel Islands Harbor's lighting system should serve practical functions. Beyond the practical, however, it should draw people to the waterfront promenade and subconsciously enhance one's experience of the Harbor. Lighting should manifest itself both as the unifying theme for Channel Islands Harbor and as the expression of its unique parts. In order for the two ends of this spectrum of expression to come together as a whole, lighting must be used as a multi-layered design tool with subtle strokes.

Existing Conditions

Illumination at the Channel Islands Harbor currently lacks uniformity in lighting levels as well as from an aesthetic standpoint. The former is a safety concern and is in need of considerable improvement. The latter should be improved to reinforce a sense of clarity and cohesion throughout the harbor. For example, as one walks along the pathways on the waterfront, one encounters several different styles of luminaire (see images below). In addition to the lack of visual continuity in the daytime, varying lamp types, optical systems, and patterns of placement create substantially different luminous environments at night (both in color of light and intensity). Measurements taken throughout the harbor range from large areas of virtually no light (levels measured



Example of existing fixtures at different promenade locations

below 0.001 footcandles), to unnecessarily high levels of illumination (pools of light from spotlights exceed 50 footcandles). Ideally, the illumination along the pathway would range from 0.25 to 10 footcandles (see Table 1 for summary of recommended illuminance guidelines), with an average of 1 footcandle.

Objectives

- Enhance feelings of safety and security throughout
- Maintain the quaint and residential feeling currently a staple of the Harbor
- Unify the Harbor from one side to the other
- Reinforce the landscape design
- Provide a memorable, distinctive effect
- Utilize equipment that is simple and straightforward to maintain
- Use readily available energy efficient lamps
- Maintain appropriate illuminance levels with performance-based fixtures.
- Maintain uniformity and marine resistant finishes considering the environment
- Meet light level recommendations set by IESNA, 9th Edition
- Respect the need for boaters to navigate the Harbor waters safely
- Minimize light pollution to support the ability to view the night sky
- Minimize luminaires susceptible to vandalism

Design Criteria

As a simple measure of light incident onto a surface, foot-candles do not describe the complete three-dimensional luminous environment. The following recommendations attempt to address the practical issues of illumination, while providing opportunities for the emotional and experiential lighting gestures that will ultimately inform, articulate, and reinforce the visual impressions of the onlooker, resident and visitor of Channel Islands Harbor. All criteria are based on recommendations from the Illuminating Engineering Society of North America (IESNA). Please refer to page 100 for a graphic illustration of the fixture types.

Table 1- Target Light Levels

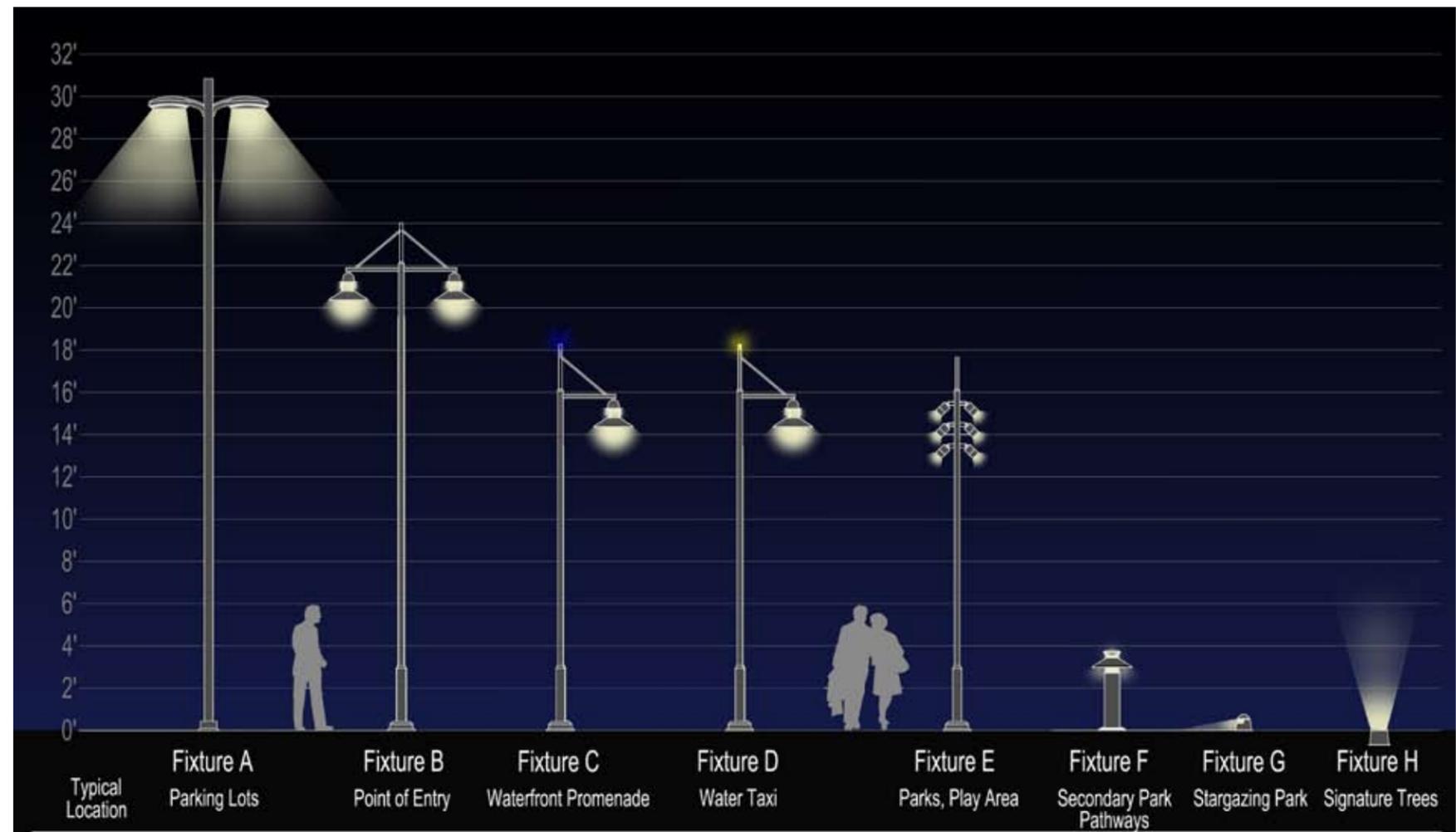
Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
A	Parking	HPS* (color-corrected)	0.2fc Minimum	Per existing poles
B	Points of Entry Victoria	MH*	0.9fc Average	75' on center
C	Waterfront Promenade	MH	1 fc average	40' on center
D	Waterfront Promenade @ Water taxi	MH	1 fc average	In specific locations where the water taxi operates
E	Parks Play Areas Special Features	MH	0.2fc Average	As needed
F	Secondary Pathways Parks	MH	0.2fc Average	15' on center
G	Stargazing Park Decks	MH	0.2fc Average	10' on center
H	Signature Trees	MH	Not Applicable	(3) per tree

*HPS- High Pressure Sodium
*MH- Metal Halide

recommendations: lighting

Why a "Fixture Family"?

The fixture family allows the flexibility of the "storytelling" throughout the Harbor while maintaining uniformity for Harbor visitors



*Refer to table 1 for more information on locations, lamp source and light level criteria for each fixture.

Lighting Improvements Plan

The improvements will focus on the main destination, the Waterfront Promenade. These recommendations will build on the success of a recently installed promenade fixture at Channel Islands Harbor Marina. This will become the standard fixture for use around the entire waterfront and will be slightly upgraded to include a subtle colored light beacon atop the mast of the fixture. Results will be a safe and pleasant promenade, with a subtle and simple “necklace” of purple/blue light which will serve as a beautiful indicator of the Harbor’s inner perimeter from the water, and from nearby inland locations.

Victoria Avenue pedestrian lighting will be significantly improved thanks to tall double-head luminaires that will light the sidewalk and the bike path. This approach will not only help unify the harbor using the same aesthetically pleasing vocabulary, but will address the safety and security concerns along Victoria Avenue.

As a visual cue for the points of entry into the Harbor property from the surrounding roadways, taller pairs of the promenade luminaire will be utilized.

Parking areas will receive new fixtures with performance-based optical systems to avoid high angle glare and improve the lighting uniformity below. Existing poles and pole locations will be used wherever possible to minimize initial cost. Quality of light will differ here from other areas, since a High Pressure Sodium lamp will be used. This source is noticeably warmer in color than Metal Halide used in all other locations. The warmth of this light will allow the Parking areas to recede in the visual background, when compared to the white light of the Harbor Entries.

Park pathways not illuminated by the promenade fixtures will receive low level bollard lights which will provide lighting to paths without competing with the Waterfront Promenade. In areas where vistas are of utmost importance, grazing luminaires will be used to define a walking path with minimal amount of light originating at ankle level. Lastly in a few locations near parks, where signature trees will be planted, they will be illuminated from below.

At Peninsula Park, where events may take place, a versatile, adjustable floodlight array is proposed. This allows for light to be directed only where it is necessary and from minimal mounting locations.

Legend

	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
	A	Parking	HPS* (color-corrected)	0.2fc Minimum	Per existing poles
	B	Points of Entry Victoria	MH*	0.9fc Average	75' on center
	C	Waterfront Promenade	MH	0.6fc Average	40' on center
	D	Waterfront Promenade @ Water taxi	MH	0.6fc Average	In specific locations where the water taxi operates
	E	Parks Play Areas Special Features	MH	0.2fc Average	As needed
	F	Secondary Pathways Parks	MH	0.2fc Average	15' on center
	G	Stargazing Park Decks	MH	0.2fc Average	10' on center
	H	Signature Trees	MH	Not Applicable	(3) per tree

recommendations: lighting

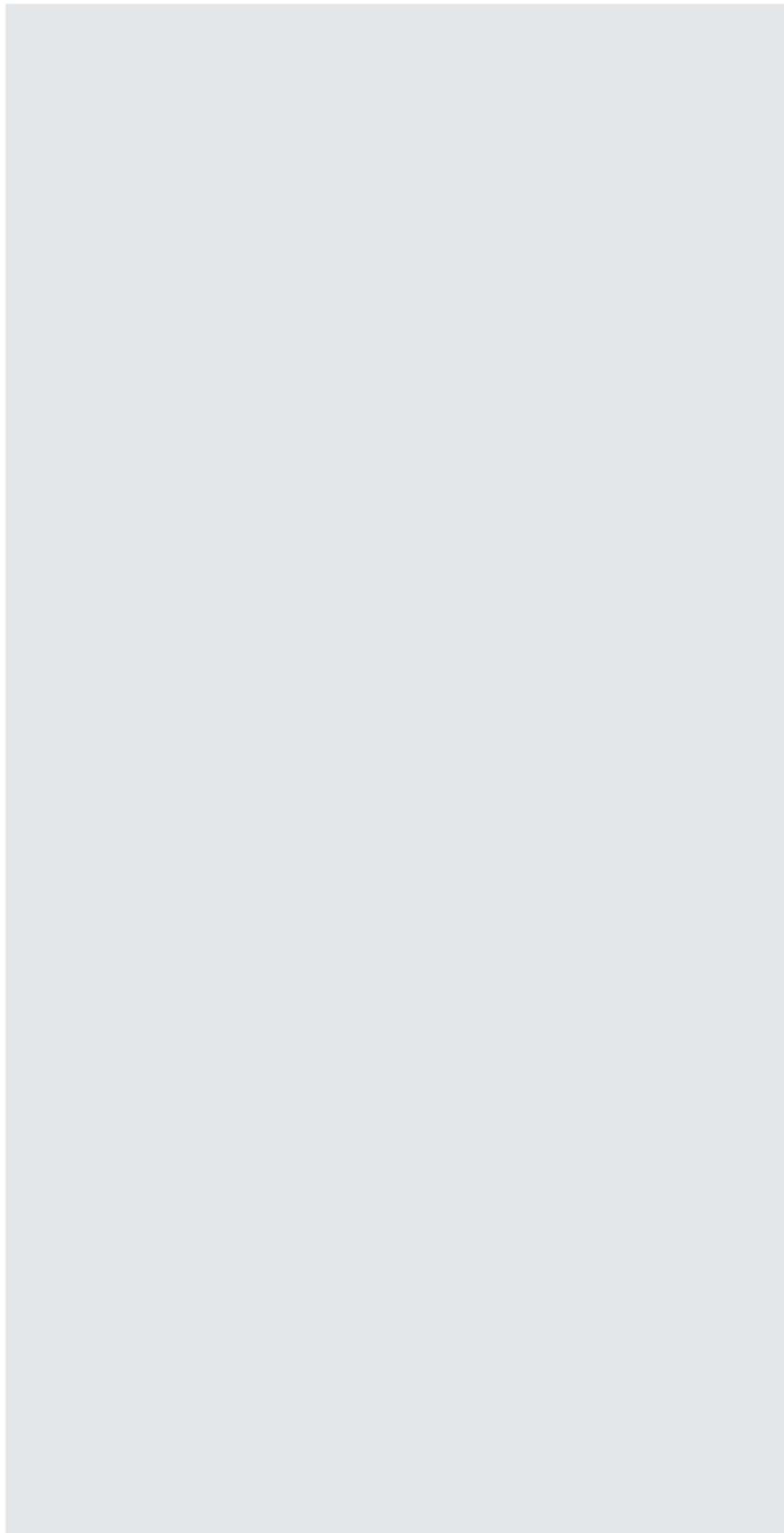


Image of proposed Waterfront luminaire with color element



Some areas of the promenade are currently do not have sufficient lighting



Points of entry, Waterfront Promenade & Water Taxi

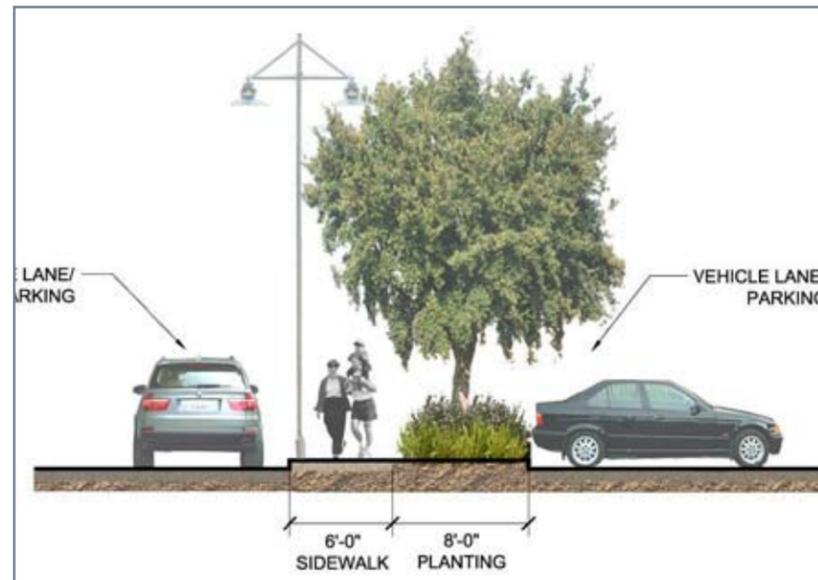


Legend

	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	B	Points of Entry Victoria	MH*	0.9fc Average	75' on center
●	C	Waterfront Promenade	MH	0.6fc Average	40' on center
●	D	Waterfront Promenade @ Water taxi	MH	0.6fc Average	In specific locations where the water taxi operates

recommendations: lighting

Double head pole mounted area / roadway luminaire at entry pathways and along Victoria Avenue



Composite image of proposed fixture at points of entry



Example of double-head fixture used at another site



Louis Poulsen Nyhavn Pole
Finish: Natural Aluminum
Arm style: Triangular (double head)

Points of entry, Waterfront Promenade & Water Taxi

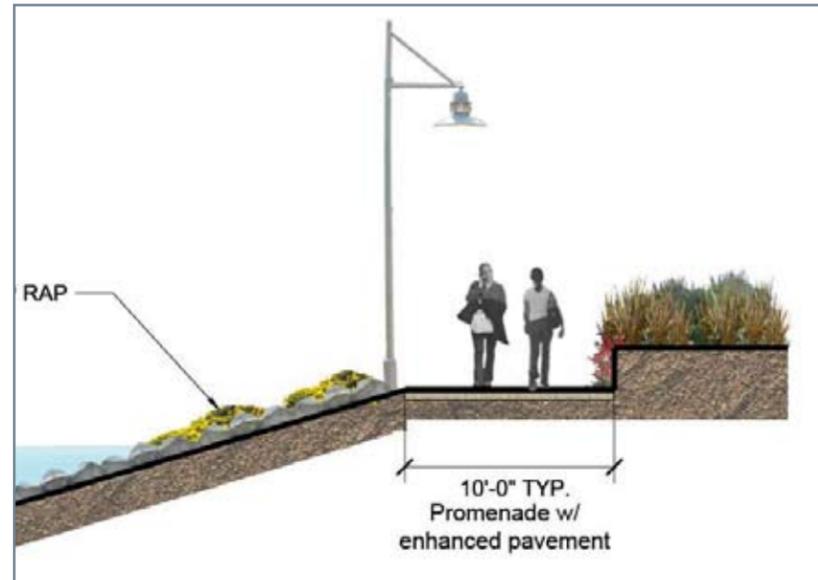


Legend

	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	B	Points of Entry Victoria	MH*	0.9fc Average	75' on center

recommendations: lighting

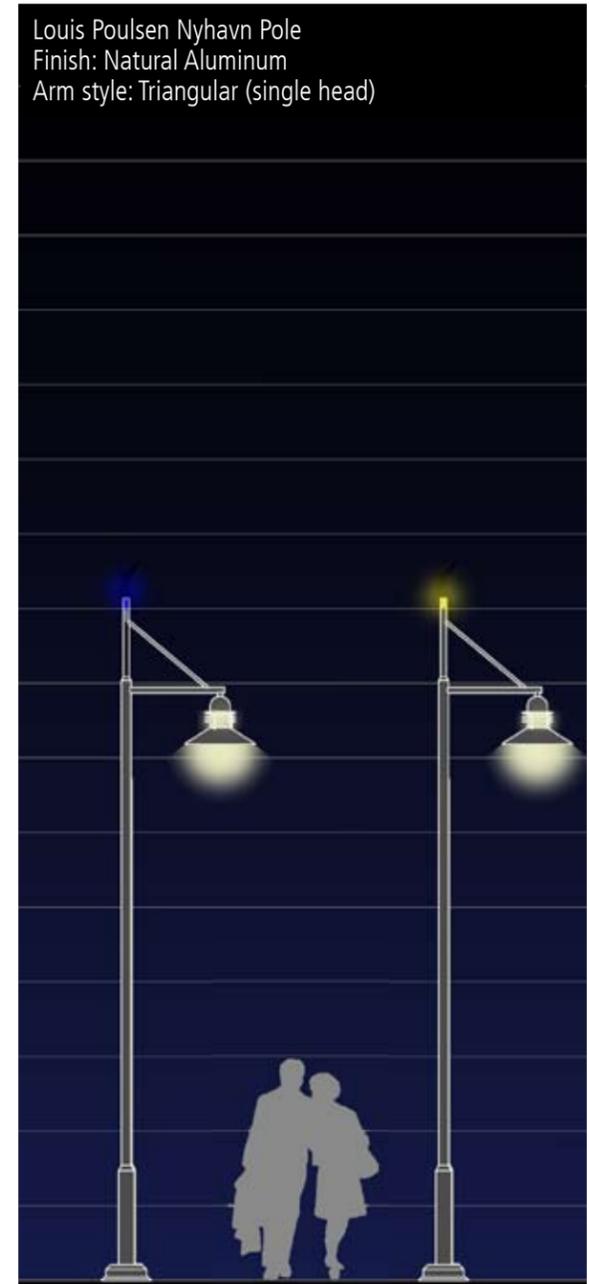
Pole mounted area / roadway luminaire with LED identification element; blue / purple for the waterfront promenade and yellow for the water taxi.



Composite image of proposed fixture at promenade



Louis Poulsen Nyhavn Pole
Finish: Natural Aluminum
Arm style: Triangular (single head)



Points of entry, Waterfront Promenade & Water Taxi



Legend

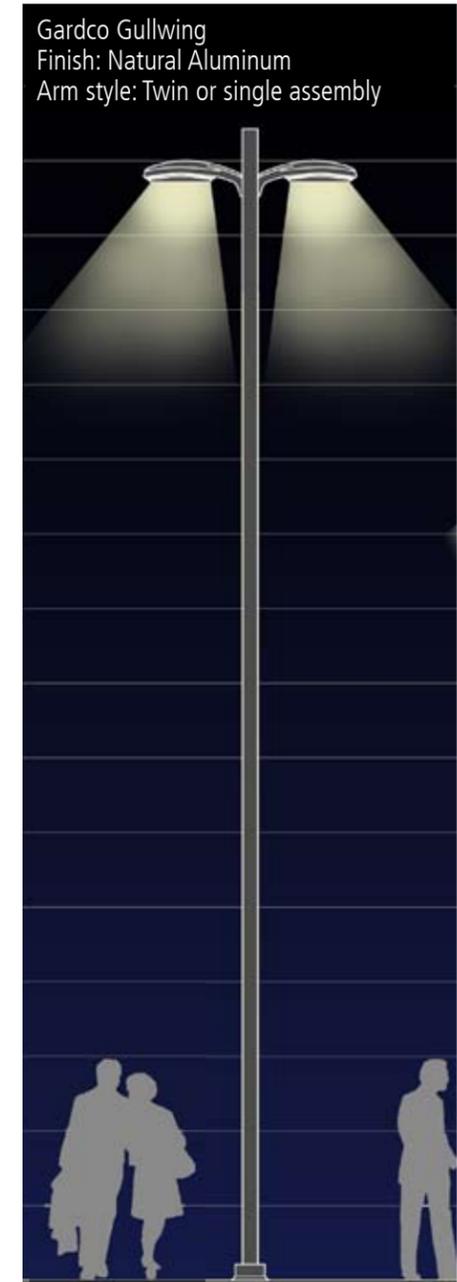
	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	C	Waterfront Promenade	MH	0.6fc Average	40' on center
●	D	Waterfront Promenade @ Water taxi	MH	0.6fc Average	In specific locations where the water taxi operates

recommendations: lighting

Performance based pole mounted area / roadway luminaire-typical for parking lots



Examples of recommended parking lot luminaires at other sites



Gardco Gullwing
Finish: Natural Aluminum
Arm style: Twin or single assembly

Parking Locations Diagram

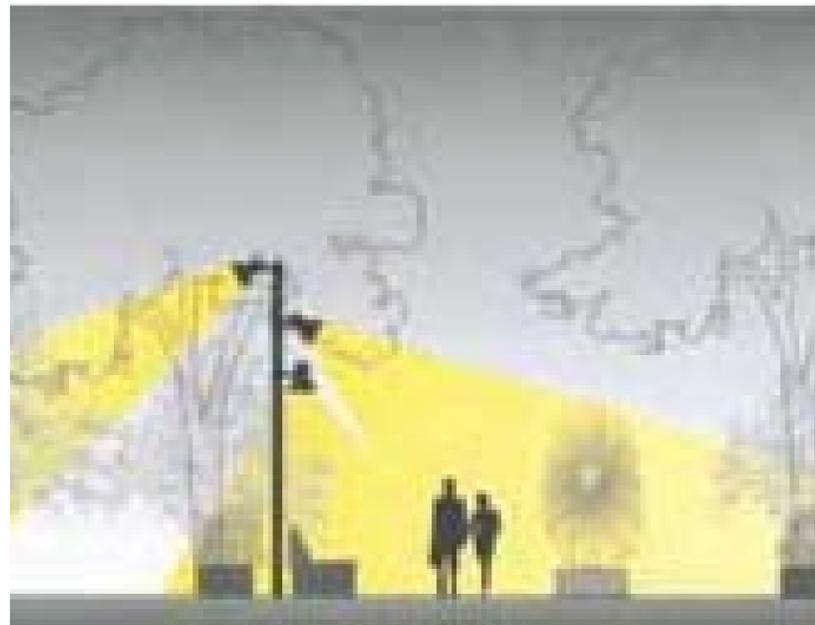


Legend

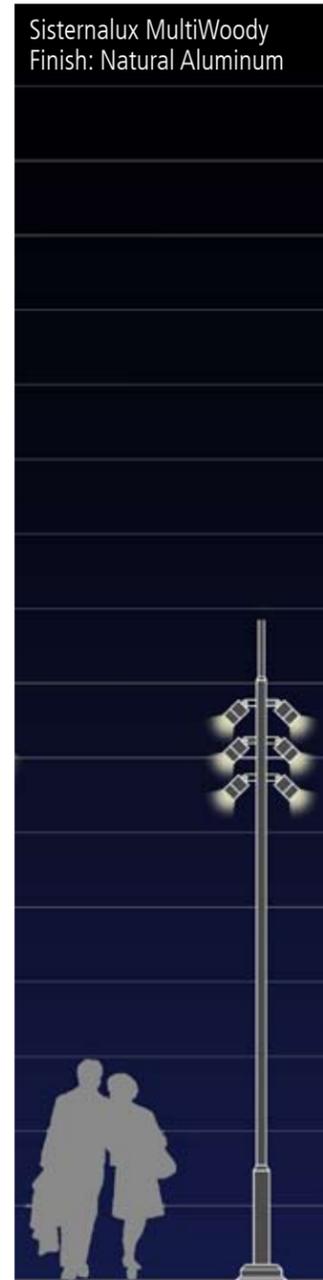
	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	A	Parking	HPS* (color-corrected)	0.2fc Minimum	Per existing poles

recommendations: lighting

Performance based fixture heads attached to pole are adjustable and will minimize cost and improve quality of light / space.



Sisternalux MultiWoody
Finish: Natural Aluminum



Parks, Decks, Signature Trees

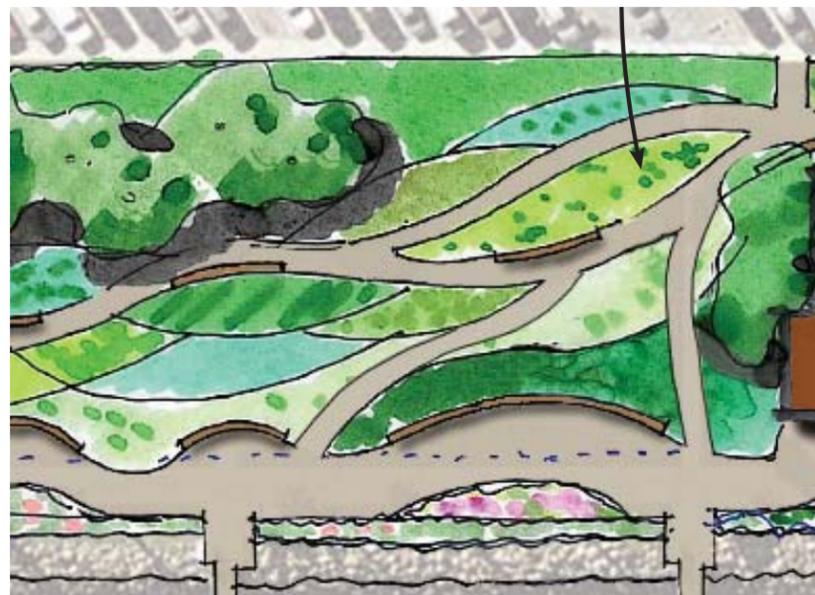


Legend

	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	E	Parks Play Areas Special Features	MH	0.2fc Average	As needed

recommendations: lighting

Ground mounted bollard at parks / secondary pathways throughout the Harbor.



Low level bollards can be located along paths to guide users at nighttime and to reinforce the landscape design

Louis Poulsen Nyhavn
Bollard
Finish: Natural Aluminum



Parks, Decks, Signature Trees

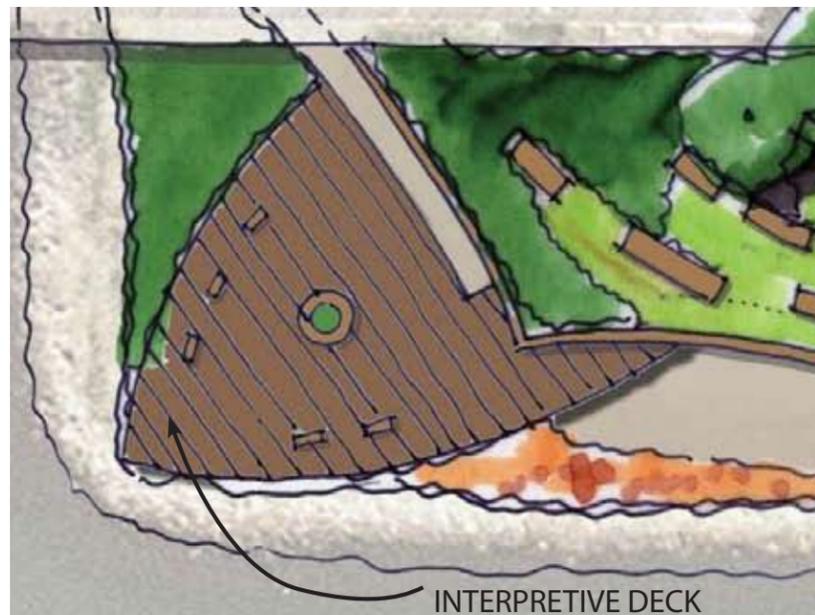


Legend

	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	F	Secondary Pathways Parks	MH	0.2fc Average	15' on center

recommendations: lighting

Ground mounted low level bollard at parks / secondary pathways reinforcing and highlighting certain elements throughout the Harbor.



Illustrative plan of park deck area where this fixture will be most suitable

Bega 8731
Finish: Eurocoat



Parks, Decks, Signature Trees



Legend

	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	G	Stargazing Park Decks	MH	0.2fc Average	10' on center

recommendations: lighting

**In-ground adjustable accent light
at signature trees that will provide
visual unity in park interpretive areas**



Example of tree highlighted by in-ground accent light

Bega 8750MH
Finish: Brushed Stainless
Steel



Parks, Decks, Signature Trees



Legend

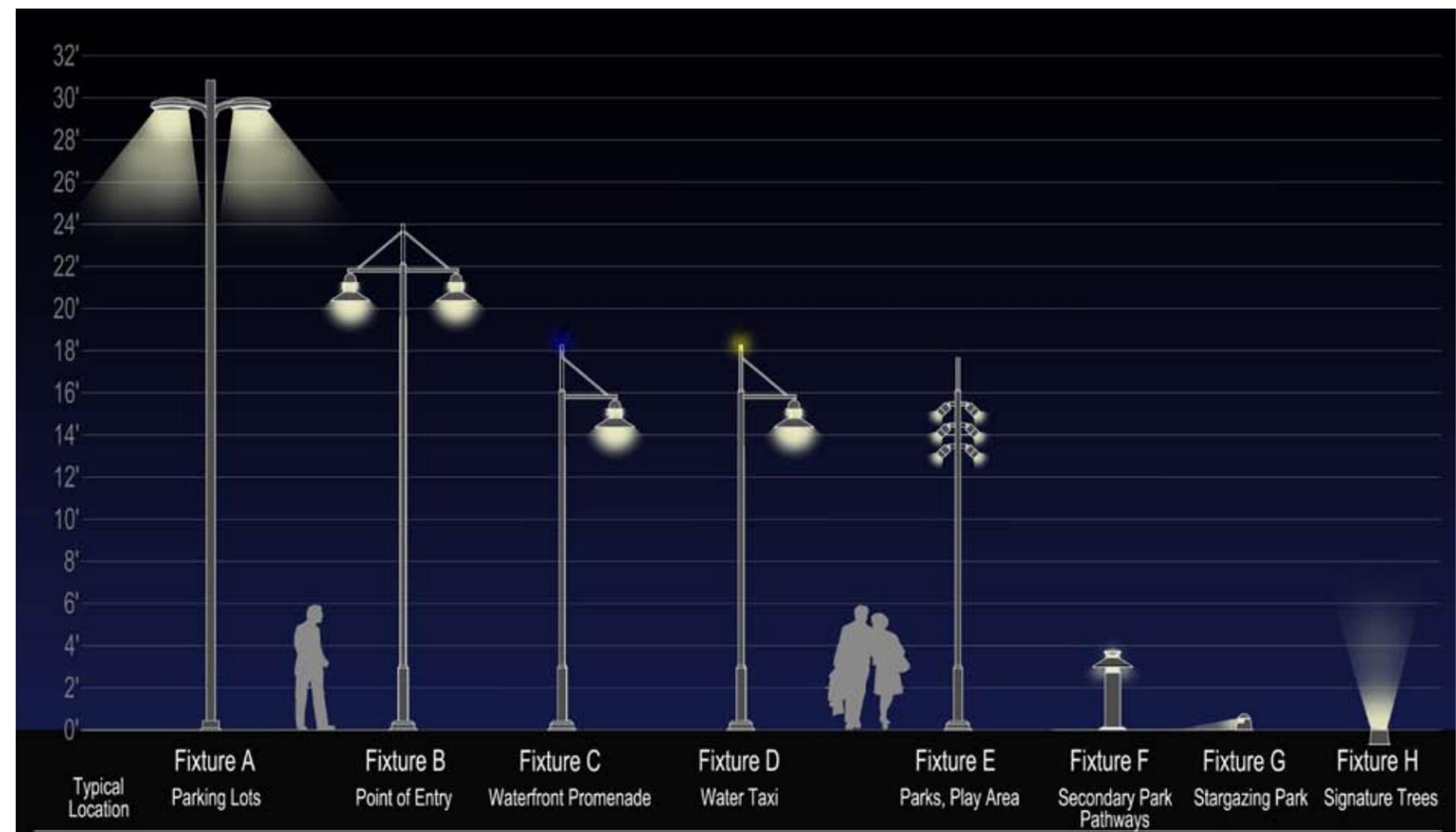
	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	H	Signature Trees	MH	Not Applicable	(3) per tree

recommendations: lighting

Lighting Improvement Plan

The overall lighting diagram presents a composite view of how the family of fixtures will be potentially located throughout the Harbor.

The overall lighting diagram presents a composite view of how the family of fixtures will be located throughout the Harbor.



Overall Lighting Diagram



Legend

	Fixture Type	Area	Source	Light Level Criteria	Typical Spacing
●	A	Parking	HPS* (color-corrected)	0.2fc Minimum	Per existing poles
●	B	Points of Entry Victoria	MH*	0.9fc Average	75' on center
●	C	Waterfront Promenade	MH	0.6fc Average	40' on center
●	D	Waterfront Promenade @ Water taxi	MH	0.6fc Average	In specific locations where the water taxi operates
●	E	Parks Play Areas Special Features	MH	0.2fc Average	As needed
●	F	Secondary Pathways Parks	MH	0.2fc Average	15' on center
●	G	Stargazing Park Decks	MH	0.2fc Average	10' on center
●	H	Signature Trees	MH	Not Applicable	(3) per tree

- Harbor waters receive runoff from the Harbor proper and other watersheds in Oxnard
- Existing conditions allow potentially polluted runoff to discharge into the harbor waters
- Current and anticipated regulations indicate a need to improve storm water management and runoff water quality

recommendations: drainage and water quality

General Observations

Storm water runoff from areas within the harbor and beyond currently discharges into the harbor waters. Existing storm drains from the City of Oxnard and elsewhere provide a conduit for pollutants to enter the harbor. These pollutants may include trash, sediment, oil, and bacteria. Stormwater runoff from Harbor parking lots, walkways, landscaped areas, building roof-tops and other areas currently flows directly into the harbor.

While control of pollutants from other jurisdictions is beyond the scope of these Design Guidelines, it is expected that adjoining jurisdictions will ultimately be required by the California Regional Water Quality Control Board (RWQCB) to take steps to improve storm water runoff quality. Therefore, it is the objective of these Design Guidelines to address storm water runoff generated within the Channel Islands Harbor jurisdiction.



Existing drain along Promenade

Existing Conditions

Parking Lots: Rain water from parking lots currently flows mostly into curb inlets, then into storm drain pipes, where it in some cases joins water from other jurisdictions, and then flows unfiltered into the harbor. Trash, sediment, oil, grease and other pollutants present in the runoff flow directly into the harbor (see figure on page 121).

Walkways: Many of the walkways throughout the harbor (e.g. the waterside promenade) are sloped to “sheet flow” directly over the rip-rap on the water’s edge and into the harbor. Other walkways drain into catch basins, where water may join other storm water and then flow directly into the harbor. Trash and debris that may have been deposited on the walkways then finds its way into the harbor waters.

Landscaped Areas: Storm water from landscaped areas is collected at inlets, usually in the planter areas, and piped to larger storm drains and into the harbor. Sediment, trash, fertilizers, and pesticides that may be present in the planter areas are transported into the harbor along with the storm water.

Building roof-tops & other areas: Rain water falling onto roof-tops, balconies, work areas, and other outdoor areas of buildings eventually makes its way into the harbor. These areas may sheet-flow onto adjoining areas, drain by down-spouts onto adjoining areas, or be piped directly into storm drain pipelines. Connection to sanitary sewers is not allowed without special permits.

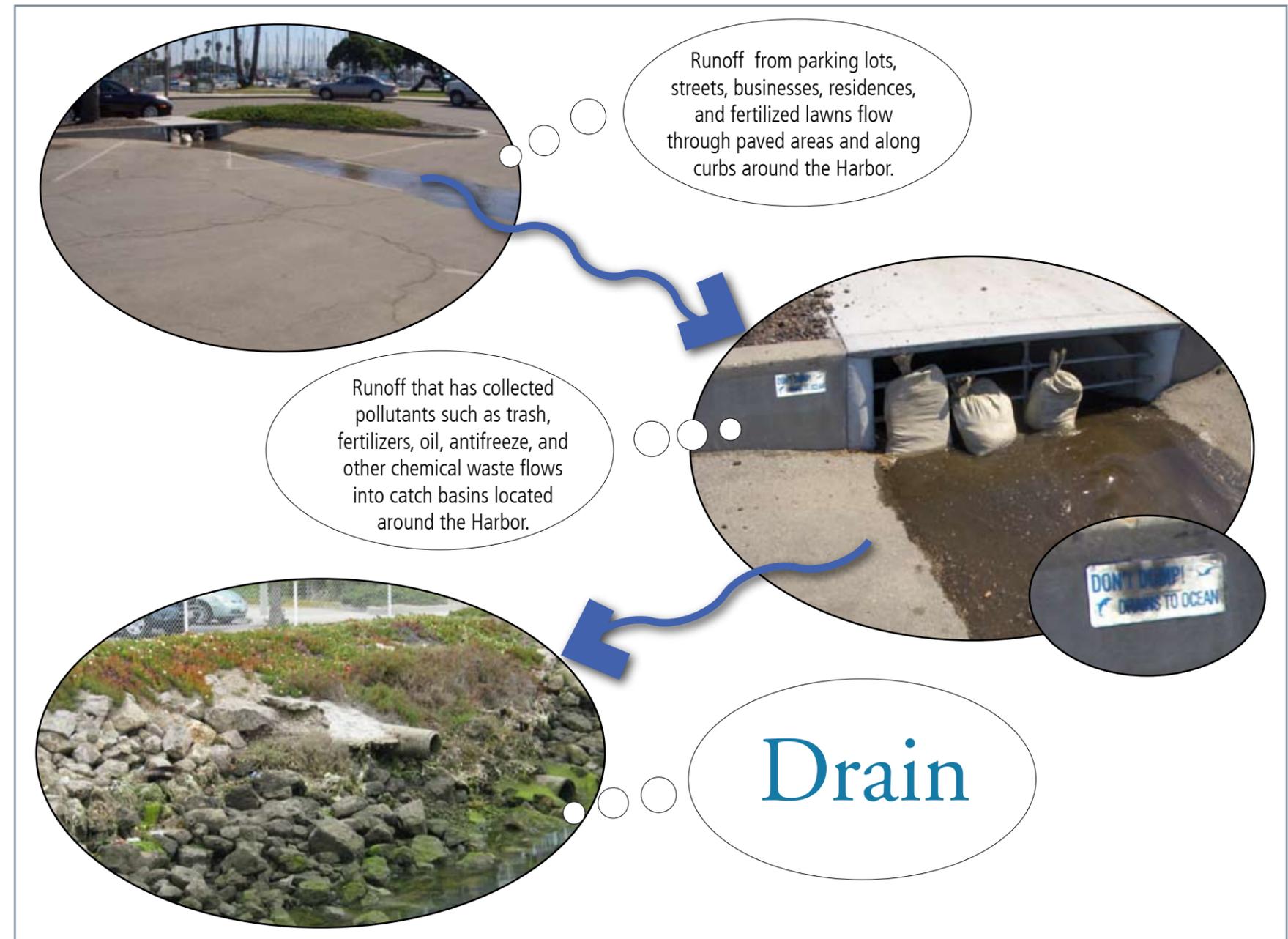
Regulatory Context

The California Regional Water Quality Control Board regulates wet weather and dry weather discharges into waters of the State. All developments within Channel Islands Harbor are subject to these regulations. New regulations are currently being developed as part of the Ventura County Municipal Separate Storm Sewer System (“MS4”) Permit.

Common pollutants include bacteria, Polycyclic Aromatic Hydrocarbons (PAHs), nitrates, pesticides, plastic by-products, mercury, lead, copper, zinc, sediment, dioxins and trash. Specific Pollutants of Concern for Channel Islands Harbor include:

- Coliform
- Chlordane
- DDT
- Dieldrin
- Polychlorinated Biphenyls (PCBs)
- Lead
- Sediment Toxicity
- Zinc

As a result of existing and upcoming regulations, all future development in Channel Islands Harbor will be subject to much more stringent regulations regarding discharges into the Harbor. As public areas are redeveloped, drainage patterns, parking lots, roadways, landscaped areas and storm drain facilities will need to be reconstructed in order to comply with current and anticipated regulations.



Storm water runoff flows from Harbor parking lots to receiving waters

recommendations: drainage and water quality

- Improve harbor water quality
- Comply with Regional Water Quality Control Board regulations
- Use combination of mechanical and biological storm water filters
- Treat runoff within each leased area
- Ongoing maintenance for improved water quality

Objectives

The overriding objective of this section is to improve water quality in the Harbor. Toward that end, the Harbor Department will require evidence of compliance with current and upcoming regulations promulgated by the California Regional Water Quality Control Board and adherence with sound environmental practices. Best Management Practices for storm water quality shall be implemented during construction as well as operation of all Harbor facilities.

Compliance with these objectives cuts across the Walkway, Parks, Landscape, Drainage & Water Quality, and Pavement, Parking & Vehicular Circulation sections of these Guidelines. Creating and maintaining public spaces, including the harbor water adjacent to them, will require attention to details in all sections of these Guidelines, as well as dedicated maintenance of all improvements once constructed.



Example of parking lot bioswale with adjacent pervious paving strip

Watershed Context

Channel Islands Harbor is the receiving water body for storm water runoff directly and indirectly for several watersheds in the City of Oxnard. Not only does storm water from areas within the jurisdiction of the County's Harbor Department drain into the harbor, but thousands of acres to the north and east within City of Oxnard watersheds drain into storm drains and open channels that discharge into Channel Islands Harbor or into the contiguous Mandalay Bay.

Some of the storm drain pipelines that convey storm water from adjacent areas into the Harbor also accept storm water from areas with Harbor Department jurisdiction prior to discharging into the harbor. It is the purpose of these Design Guidelines to improve storm water quality within Harbor Department jurisdiction; water originating in other jurisdictions will be regulated by others.

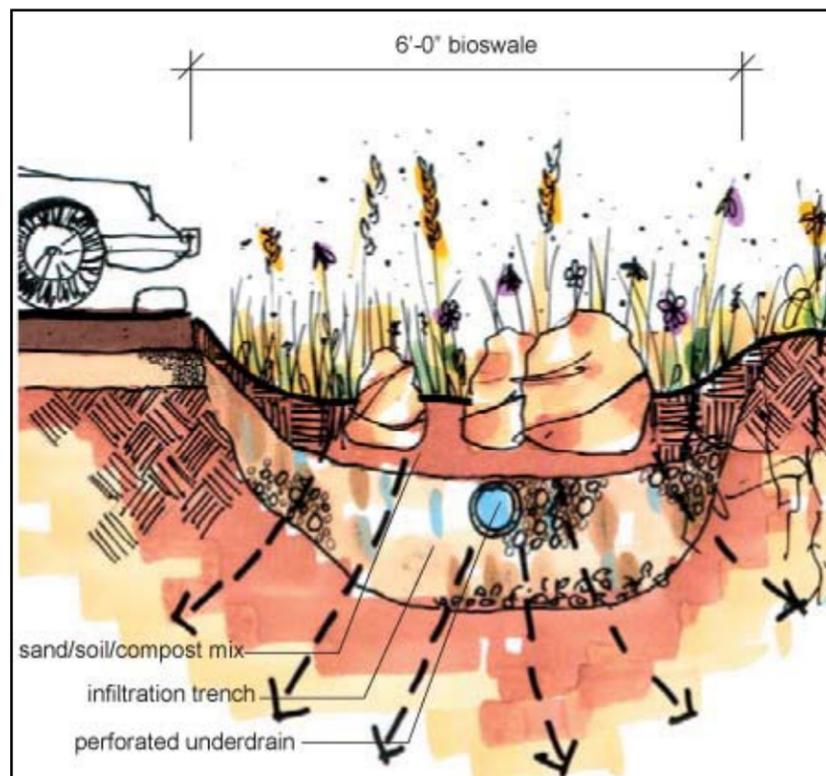


Example of grassed bioswale

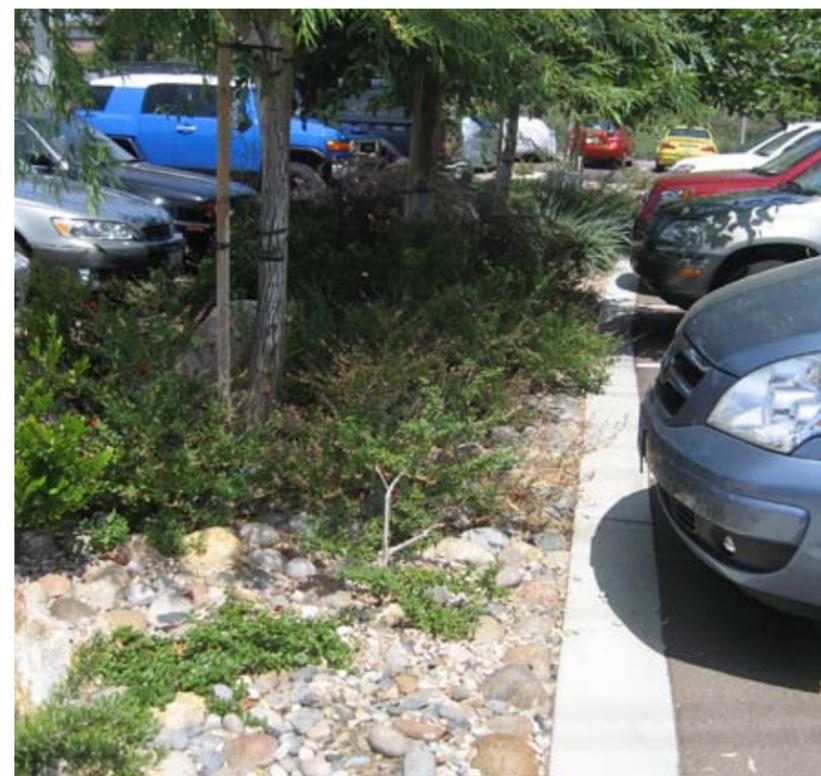
Design & Operations Criteria

- Grading and drainage plans shall be signed and stamped by a California registered civil engineer.
- Storm water management shall comply with current and reasonably foreseeable Regional Water Quality Control Board requirements.
- Storm water runoff from lessee areas shall be treated by lessee within leased area prior to discharge unless approved otherwise in writing by the Harbor Director.
- Lessee operations shall comply with current “Clean Marina” guidelines.

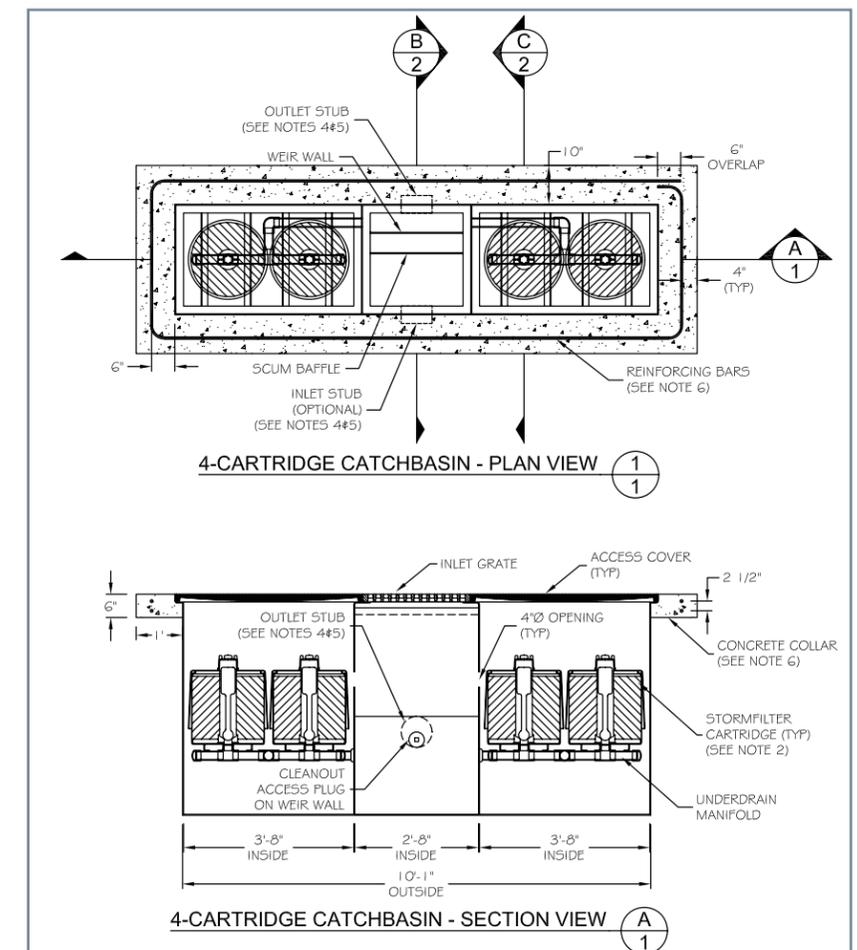
- Storm water filters shall be maintained regularly by the entities responsible for the various filters (varies by location), with maintenance records provided to the Harbor Department.
- Parking lots, walkways, balconies, roof-tops and landscaped areas shall be swept, cleaned and maintained regularly by the entities responsible for the various locations (varies by location), in accordance with lease terms or other procedures developed by the Department. Work areas such as boatyards will require special filtration and permitting by other agencies. Copies of such permits and evidence of compliance and adequate maintenance shall be provided to the Harbor Department.



Conceptual diagram of parking lot bioswale



Example of parking lot bioswale without curbing



Sample plan and section drawings of a mechanical filter

recommendations: pavement, parking & vehicular circulation

- Deteriorated pavement is unsightly and impacts water quality
- Several entities are responsible for maintaining paved areas
- Excessive pavement poses unnecessary maintenance burden and has environmental impacts
- More efficient vehicular circulation can improve appearance and safety

General Observations

Much of the asphalt concrete pavement throughout the harbor is reaching the end of its useful life. Currently, of the nearly 3.7 million square feet of asphalt concrete pavement, maintenance responsibilities are apportioned approximately as follows (see figure on page 125):

Lessees:	42%
City of Oxnard:	34%
County & Lessees (joint):	23%
County	1%

Maintenance of the large areas of asphalt throughout the harbor has become a financial burden and the deteriorated condition contributes to stormwater siltation and otherwise detracts from the overall appearance of the harbor.

Existing Conditions

The existing asphalt concrete suffers from a lack of routine preventative maintenance. Further, some areas appear to have more paved surfaces than are necessary to accommodate parking and access demands of adjacent land and water parcels.

Vehicular circulation can be improved by eliminating some of the more circuitous routes through parking lots. Pedestrian safety can be improved by eliminating unnecessary drive aisles within parking lots, and providing landscaped points of refuge for pedestrians.



Existing driveway near Public Boat Launch



The Harbor's west side has excessive surface parking areas with little landscaping

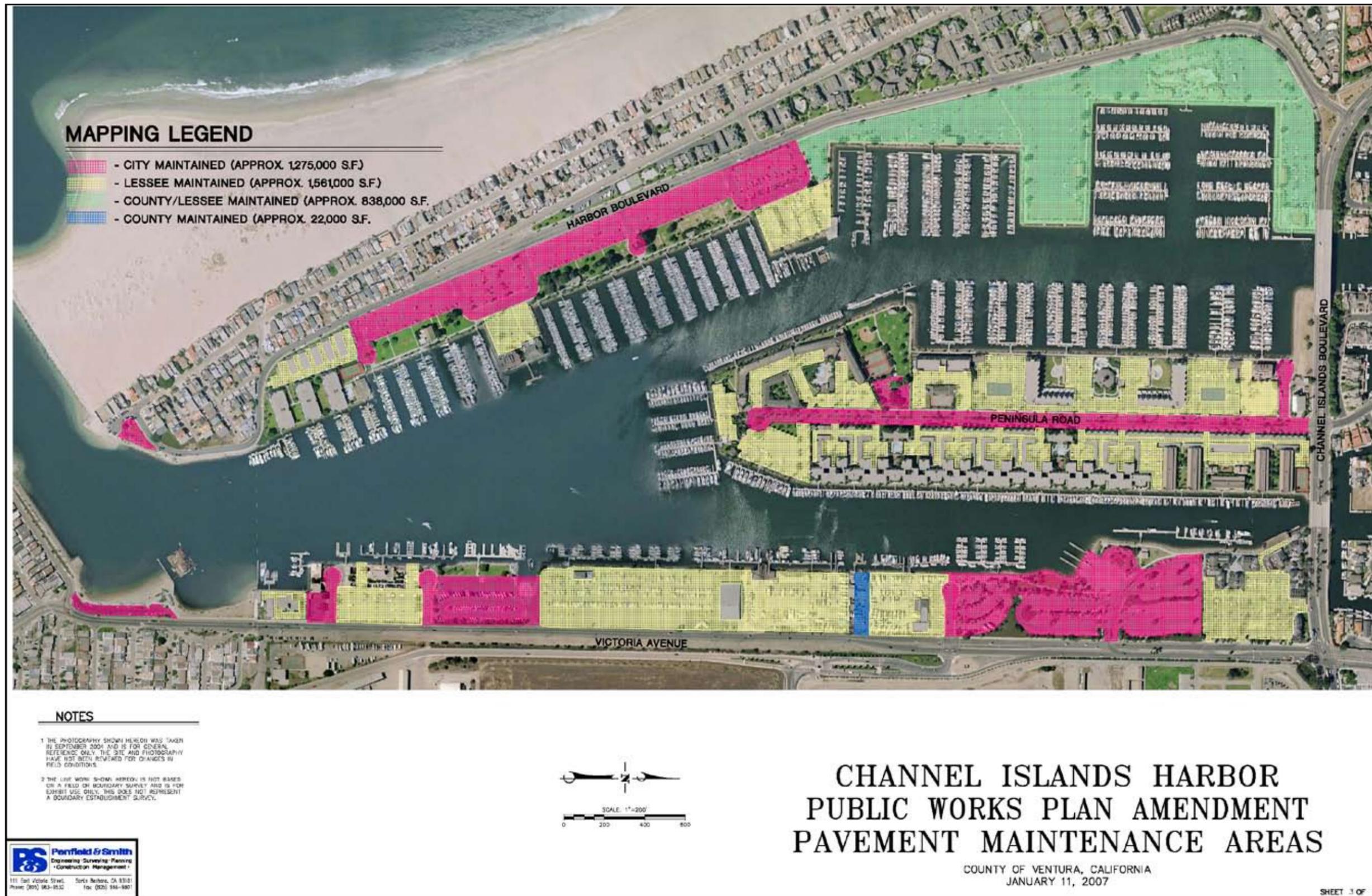


Diagram showing pavement maintenance responsibilities in Channel Islands Harbor

recommendations: pavement, parking & vehicular circulation

- Improve visitor safety and comfort
- Remove unnecessary internal streets and driveways
- Reduce impervious surface area
- Increase vegetated areas
- Provide adequate parking in the future

Objectives

The intent of these Guidelines for pavement, parking, and circulation is to ensure that access to the Harbor for both vehicles and pedestrians is provided where desirable and warranted. Both design and maintenance requirements are included.

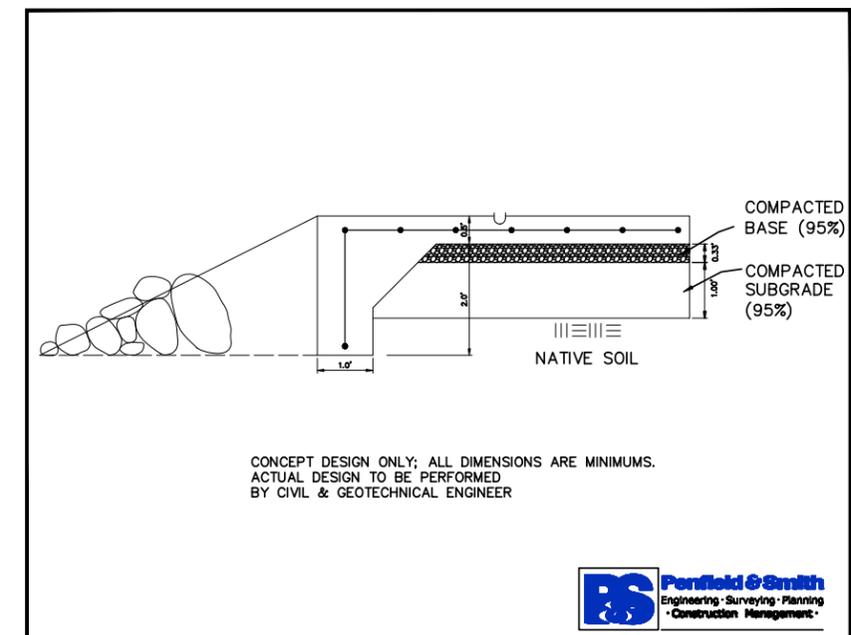
Unnecessary internal roadway/driveways within parking lots can be eliminated and drop-off points near marina gangways and businesses can be created. Reductions in vehicle speeding and increased pedestrian safety can be obtained by eliminating long unobstructed corridors within parking lots.

In addition, a reduction in storm water runoff as well as elimination of stormwater pollutants is desired. Therefore, use of pervious pavements and elimination of unnecessary impervious surfaces is desired.

In areas with a documented surplus of parking, conversion of existing paved surfaces to pervious surfaces such as landscaping and/or vegetated swales will reduce stormwater runoff, improve physical appearances, and reduce heat stored and later emitted by black asphalt. Conversely, parking studies will be required of future land use changes to assure that adequate parking is provided for all parcels within the harbor.

Design & Maintenance Criteria

Pavement: The pavement for public walkways should be constructed for aesthetics, durability, and a 25-year design life. Asphalt and concrete pavement shall be designed to accommodate reasonably foreseeable vehicle loads (e.g. H20 highway loading), and shall be specified by a California registered Civil Engineer, in consultation with a California registered Geotechnical Engineer.



Conceptual section of concrete sidewalk along waterfront. Width varies from 6 ft to 14 ft. Actual thickness, reinforcement, base and subgrade preparation and concrete mix shall be determined by registered Civil and Geotechnical engineers, and shall be submitted for review to Harbor Department. Sidewalk design shall comply with City of Oxnard requirements when applicable. See corresponding text for additional guidelines.

For concrete walkways not exceeding 15 feet in width, along the waterfront, the design shall include the following unless approved otherwise in writing by the Harbor Director.

- Doweled expansion joints at 10-feet on center
- Crack control joints at 5-feet on center, each direction
- 4500 psi concrete strength
- Type II cement, 7½ sack minimum
- 0.40 water/cement ratio
- 15% fly ash content
- 10-day moist cure with curing compound
- Green epoxy-coated reinforcing steel 60 ksi No. 4s @ 12 inches each way
- ¾-inch chamfer on exposed edges

Concrete driveways and walkways wider than 15 feet shall require independent design, depending on anticipated vehicle loads.

Preparation of subgrade for asphalt and concrete pavements shall be observed by a California registered geotechnical engineer. The geotechnical engineer may modify the design requirements depending on his/her observations.

Parking lot landscaping: Landscaping shall be included in parking lots as follows, unless approved otherwise by the Harbor Director.

- For every ten parking spaces in single-loading parking bay row, at least one perpendicular or diagonal landscape planter is recommended, having minimum dimensions of 9 feet by 20 feet measured from outside curb face to outside curb face. For every 20 parking spaces in double-loaded parking bays at least one landscape planter is recommended, having minimum dimensions of 9 feet by 40 feet measured from outside curb face to outside curb face. Configurations other than perpendicular or diagonal planters may be considered.
- Landscaping shall be irrigated with an automatic irrigation system per guidelines in the Landscape section on p. 56.
- Plant material should be selected from Tables on pp. 58-69 in the Landscape section; recommended “Streetscape” and “Bioswale” species are more suitable for parking lots (with “Bioswale” species used in areas identified for biofiltration).
- Trees should be used to frame view corridors and direct people to Harbor entry points.
- Earthen berms and existing topography shall complement landscaping as a component of the landscape plan.
- Where curbing delineates a traveled way, the finished grade of the planted area shall be at least three inches below the top of the planter curbing.

- Landscaped bioswales and biofiltration areas shall have no curb or discontinuous curbing to allow water to enter.
- All plant material within a 30-foot triangle at the intersection of two streets shall be no more than three feet in height above curb level at maturity.
- Landscaping at parking lot entrances/exits shall not hamper the line of sight of drivers of vehicles entering or leaving parking lot areas.
- Low walls, landscape berms, hedges, or other features may be used to screen vehicles from public streets.



recommendations: pavement, parking & vehicular circulation

Parking: The number of parking stalls for each lease parcel shall be consistent with the Channel Islands Harbor Parking Demand Study available from the Harbor Department. For land uses not contemplated by said study, lessee shall provide a supplemental study acceptable to the Harbor Director and signed by a California registered Traffic Engineer.

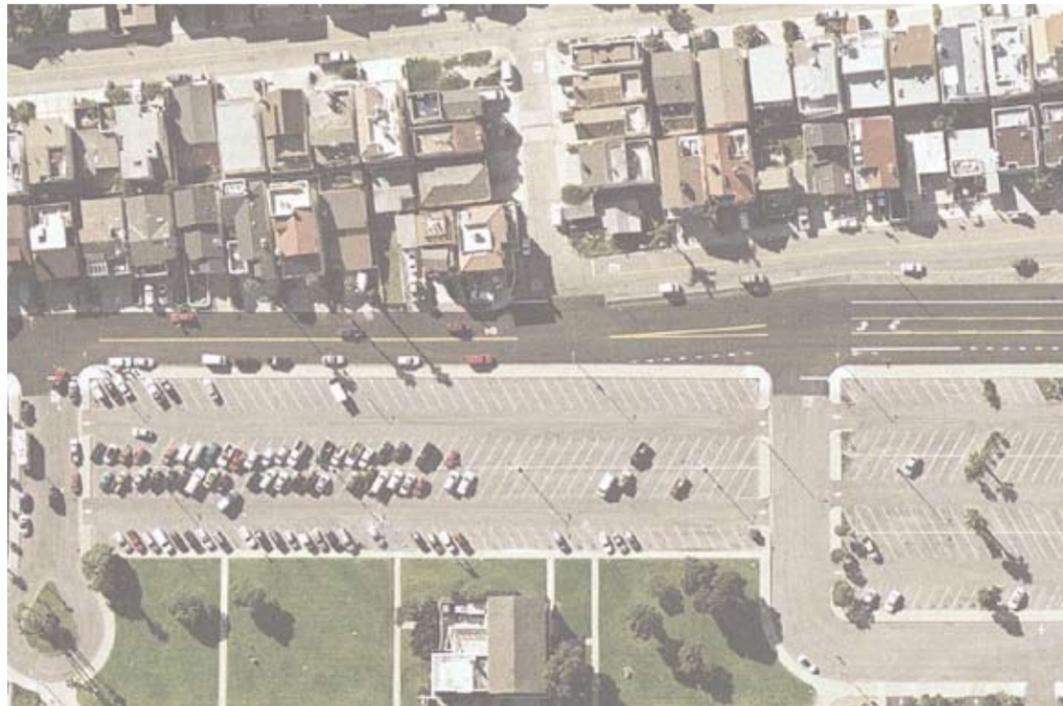
- Connections with City streets shall be reviewed by the City of Oxnard Traffic Engineer.
- Parking lot layout shall minimize paved area, while maintaining 9-foot wide stalls in at least 80 percent of the total stalls in the parking area (as designated by the Harbor Department). The wider stalls shall be nearest the marina gangways.
- Areas to be used for overflow parking, as well as landscaped areas, shall be pervious and shall be utilized for biofiltration of stormwater whenever feasible.
- Parking lot plans shall be signed and stamped by a California registered traffic or civil engineer.
- Asphalt and Portland cement concrete pavement design sections shall be engineered by a California registered civil engineer.

The figures on page 129 present a general concept for the arrangement of bioswales/biofilters in parking lots on the Harbor's west side. There is an excess of parking here; the bottom diagram shows one option for the distribution of bioswales (it does not show other landscaped areas in parking lots). The upper two figures present a conceptual view (before and after) of bioswale/biofilter location in one parking area on the west side of Channel Islands Harbor. The ultimate sizes of the biofilter

areas depend on the parking capacity needed according to the Parking Demand Study. Final bioswale/biofilter locations will also depend on parking lot grading.

Maintenance: Asphalt concrete pavement shall be evaluated annually by a California registered Civil Engineer, with the results of this evaluation presented to the Harbor Department. All entities with pavement maintenance responsibility in the Harbor are encouraged to collaborate on both the annual evaluation and the resulting preventative maintenance and rehabilitation.

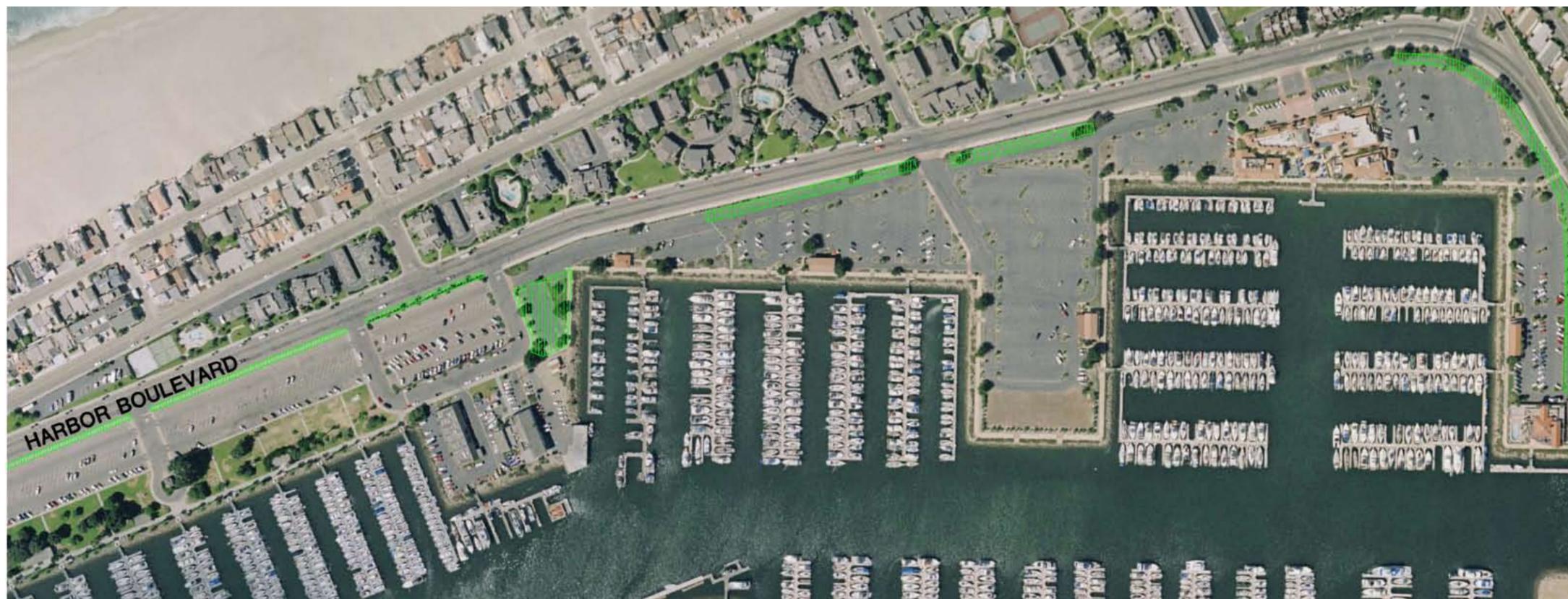
It is expected that a program of crack sealing, seal coating (fog seal, chip seal, slurry seal, cape seal, etc.), and overlays be implemented to address signs of pavement distress prior to failure of the pavement and resulting water intrusion and potholing.



Existing condition



Biofilters and other landscaping in solid green marking



Top: conceptual diagrams showing locations of landscaped areas in a portion of a parking lot. Bottom: conceptual diagram showing possible parking lot biofilter/bioswale areas (highlighted with green hatching) for the west side of Channel Islands Harbor

implementation

The recommendations for public enhancements can be incorporated with pending redevelopment projects currently in planning phases, and can guide long-term improvements as well. Because the County of Ventura owns and operates Channel Islands Harbor, it is responsible for overseeing implementation of public enhancements. Much of the plan implementation will be carried out by Harbor lessees, who enter into long-term lease agreements with the County of Ventura, and with the remainder being the responsibility of the Channel Islands Harbor Department. Because many of the public parks are within the City of Oxnard, the City is a likely partner to participate in rebuilding these public areas.

Any construction or reconstruction of leased areas will be guided by these design standards. Each operator will develop a Preventative Maintenance Plan to address pavement, pavement markings, signage, lighting, storm drains, stormwater filters, and landscaping. Pavement maintenance plans should be updated every five years.

Entitlements/Permits

Development in the Harbor is governed by the Channel Islands Harbor Public Works Plan (PWP) originally approved by the Coastal Commission in 1986. The PWP has been amended several times to accommodate construction or reconstruction of facilities around the Harbor. Projects that are specified in the PWP require the filing of a Notice of Impending Development (NOID) with the Coastal Commission, which then has limited authority over the process. Projects not included in the PWP must be submitted to the Coastal Commission through a PWP amendment.

The PWP has, from its inception, accommodated a promenade around the Harbor, parallel to the water wherever possible. The PWP also identifies areas of the Harbor that are set aside for parks, parking, and public amenities. Where redevelopment will be carried out consistent with the PWP, the Plan and Design Guidelines can be implemented



through the NOID process with the proposed project. Where parks are proposed for expansion, or where facilities are proposed that are not specifically spelled out in the PWP, an amendment to the PWP may be required.



The landside area of the Harbor is within the City of Oxnard; however, the water area is located within the County of Ventura. An agreement between the County and the City, signed in 1963, gives planning authority for new facilities to the County of Ventura; the City has permitting authority. Issues such as fire protection, public safety, storm water quality management, and building code compliance are regulated through the City process. In addition, under the 1963 agreement, the City of Oxnard has responsibility for maintaining about one-third of the public parks and parking areas, and the public streets. Approval and permitting for improvements to these areas will be coordinated closely with the City.

Enforcement

The design criteria and guidelines for the Harbor's public improvements on lease parcels are implemented by the County of Ventura through lease agreements with Harbor lessees. These lessees have agreed to adhere to guidelines for public areas adopted by Ventura County as they rebuild their leaseholds. All development plans will be reviewed by Harbor Department staff for compliance with these guidelines before permits are obtained.

Phasing

Several parcels in the Harbor will undergo redevelopment in the near term, while other areas do not anticipate significant change for a decade or more. Near-term redevelopment projects will generally follow the approved guidelines for public walkways, landscaping, signage, lighting,

implementation

parking, and drainage. Because other parcels will not have near-term improvements associated with lease agreements, the discontinuous nature of implementation presents a challenge to the goals of enhanced public access and associated upgrades in lighting, signage, and landscaping. The following points discuss noteworthy issues and possible resolutions:

- **Waterside promenade:** The goal of a continuous promenade is achievable along much of the Harbor's water edge before 2015. After near-term redevelopment projects are completed, the most significant gaps in the waterside promenade will occur at Paz Mar Select Apartments on the west side, and Paz Mar Reserve Apartments on the peninsula. An interim connection between the promenade to the north and south of Paz Mar Select Apartments already exists as a sidewalk along Harbor Boulevard. For Paz Mar Reserve Apartments, a sidewalk exists along Peninsula Road immediately to the west; it is important that the redevelopment of the Casa Sirena parcel at the end of the peninsula incorporate a pedestrian linkage between its waterside promenade and the sidewalks on Peninsula Road. Other gaps in the waterside promenade occur at Mandalay Bridge Park and Harbor East Park—these two areas currently lack an improved path. Because the parks are owned by Ventura County, their improvements are not necessarily associated with lease agreements and lessee redevelopment projects. It is the goal of the Harbor Department to construct the waterside promenade for these two parks within two years of adjacent promenade establishment, to ensure timely provision of a continuous promenade in these areas.

- **Promenade enhancements:** Some parcels have an existing waterside promenade, with no plans to rebuild or improve the promenade in the near term. To contribute to goals of increased design cohesion, safety, and wayfinding in the Harbor's public realm, recommendations to enhance and update promenade lighting, signage, and landscaping should be implemented before any walkway repair or replacement is needed. It may be more cost-effective for associated lessees to contribute to a common fund for these enhancements, which could then be installed by the Harbor Department in a few large phases.
- **Victoria Avenue walkway:** After near-term redevelopment, a walkway should be present along much of Victoria Avenue, except for certain parcels currently held by the County. It is desirable to extend the



sidewalk the entire length of Victoria Avenue adjacent to the Harbor as soon as possible. One option is to coordinate construction in the “gap” parcels with adjacent parcel redevelopment, with funding from Ventura County and/or the City of Oxnard.

- Gateway signage: Improved identity/gateway signage (and associated lighting and landscaping) will benefit all Harbor lessees by identifying the Harbor as a destination. Installing new gateway signage at the outset of the redevelopment process will announce that real improvements are underway as the near-term projects proceed. Funding sources for new gateway signage are unknown at this point, but critical to a cohesive program.
- Parks: Construction of park improvements is expected to occur over a long time period, as determined by availability of funding to the Harbor Department and/or City of Oxnard. Some

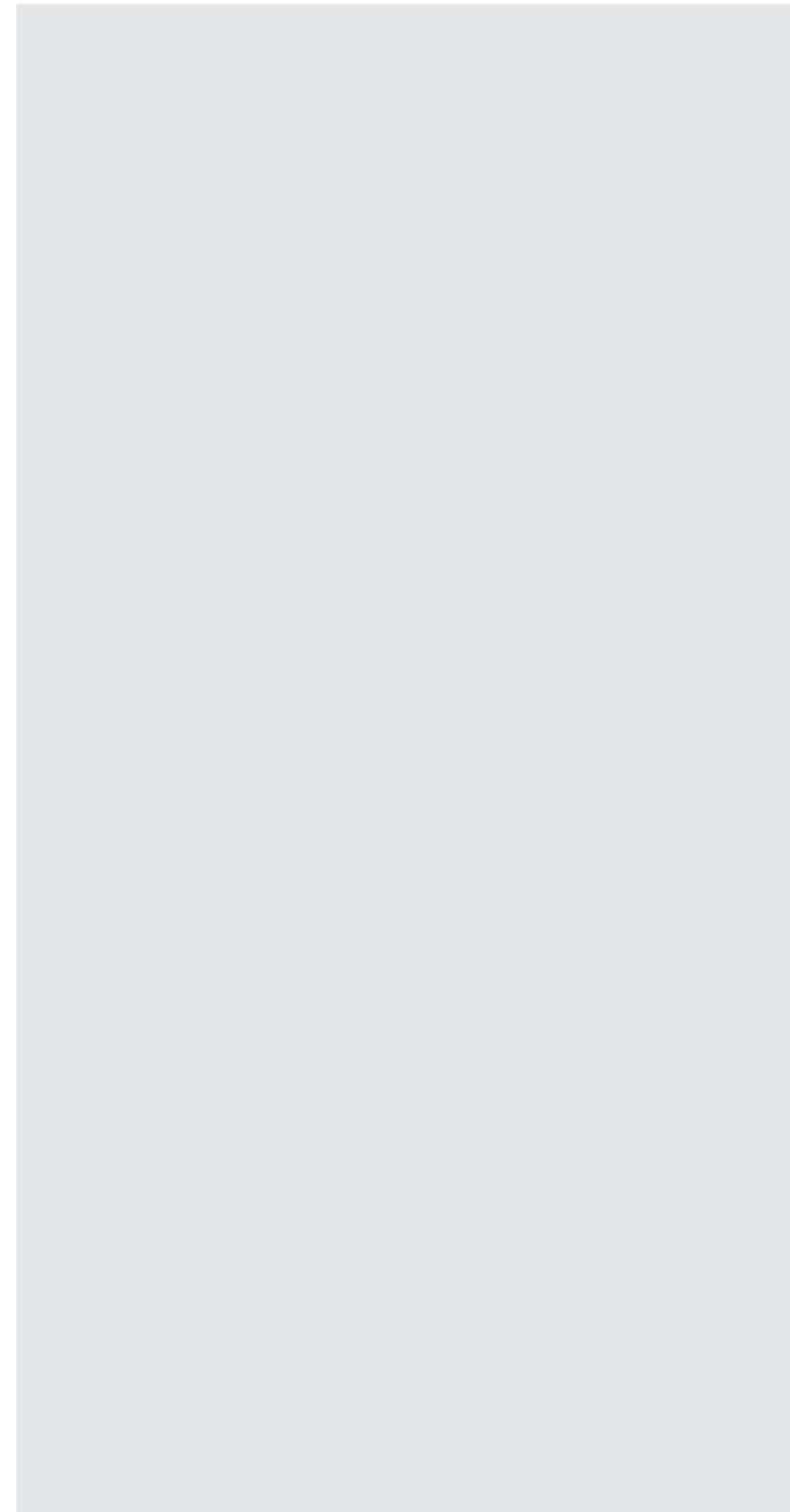


landscape improvements (e.g., replacing trees or site furniture with recommended plants or furniture) should be coordinated with the City of Oxnard, which maintains the Harbor’s parks. Priority should be given to park improvements that:

- Generate excitement about recreation and interpretive opportunities in the Harbor’s parks
 - Increase children’s play opportunities, especially on the east and west sides of the Harbor (which currently lack play equipment)
 - Improve the visibility of the Harbor and enhance the viability of its businesses
 - Can be constructed in association with another project on or near the park (e.g., changes to parking areas or walkways)
- Parking lots: Most of the parking lots on the Harbor’s west side (plus parking for the public boat launch on the east side) are the responsibility of the City of Oxnard. Given funding limitations, replacement of parking areas must be prioritized to achieve maximum results. The following criteria can help set these priorities:
 - The paved surface is significantly deteriorated
 - The improvements can help the Harbor meet regulatory standards for water quality or other environmental concerns
 - The improvements will facilitate changes for adjacent areas (e.g., parking or circulation changes on an adjacent parcel)
 - The improvements can be combined with an adjacent project at a cost savings



appendices



existing conditions: public walkways

In general, the walkways—whether along the waterfront or along the streets—lack uniformity, do not provide complete access to the water’s edge, and in some cases do not comply with current accessibility requirements for the disabled. The existing promenade around the Harbor is not continuous, with access in some places restricted by private development and land use (see figure summarizing existing conditions on page 137). In some areas there is no walkway at all, particularly on the east side of the Harbor. Some areas provide little space between buildings and the water, which constrains promenade width. The concrete path is typically 10 feet wide and is aligned in a straight pattern with few enlarged areas or other places along the way to encourage pausing; it currently lacks frequent seating for users to enjoy the wonderful views. There is a broad mix of edge treatments, including plantings, rip-rap, retaining walls, and bare slopes. A lack of aesthetic cohesion presents confusion to visitors.

A bridge on Channel Islands Boulevard provides a public sidewalk on its south side. It is the only pedestrian linkage among the Harbor’s west, peninsula, and east sections. This walkway is relatively narrow, with limited ability to be widened. Currently, Victoria Avenue does not have a continuous walkway, although pedestrians and bicyclists frequently travel along this street. The high vehicle speeds on Victoria Avenue make it dangerous and unpleasant for users, including children who walk or ride their bikes from the Silver Strand neighborhood to Hollywood Beach Elementary School on the west side of the Harbor.

On the Harbor’s east side, active boatyard facilities interfere with pedestrian access along the water’s edge for these parcels. Because the boat lifts straddle the land-water interface, there is no option to conveniently route pedestrian traffic around the boat lifts without intruding on boatyard operations and creating a public safety hazard. In addition, the County boatyard services Harbor Patrol boats, and security concerns preclude public access to this area.



Diagram of existing walkways in Channel Islands Harbor

existing conditions: parks



Channel View Park



Harbor View Park

The Harbor's major parks include Channel View Park (on the west side, at the south end of Harbor Boulevard), Harbor View Park (a linear park along the Harbor's western edge), Peninsula Park (on the west side of the Harbor's peninsula, between the Casa Sirena Hotel and Anacapa Isle Marina), and a small linear park on the Harbor's east side (south of Anacapa Yacht Club). This last park is not named; it will be referred to as "Harbor East Park" for convenience. In addition to the four existing parks, a vacant parcel at the northwest end of the peninsula can be developed as a fifth park space (referred to as "Mandalay Bridge Park" for convenience). Several places along the waterside promenade offer exceptional views of the Harbor; one near the public boat launch already has seating to allow visitors to enjoy the view. These places may be further developed as pocket parks to encourage small gatherings near the water.



site for Mandalay Bridge Park

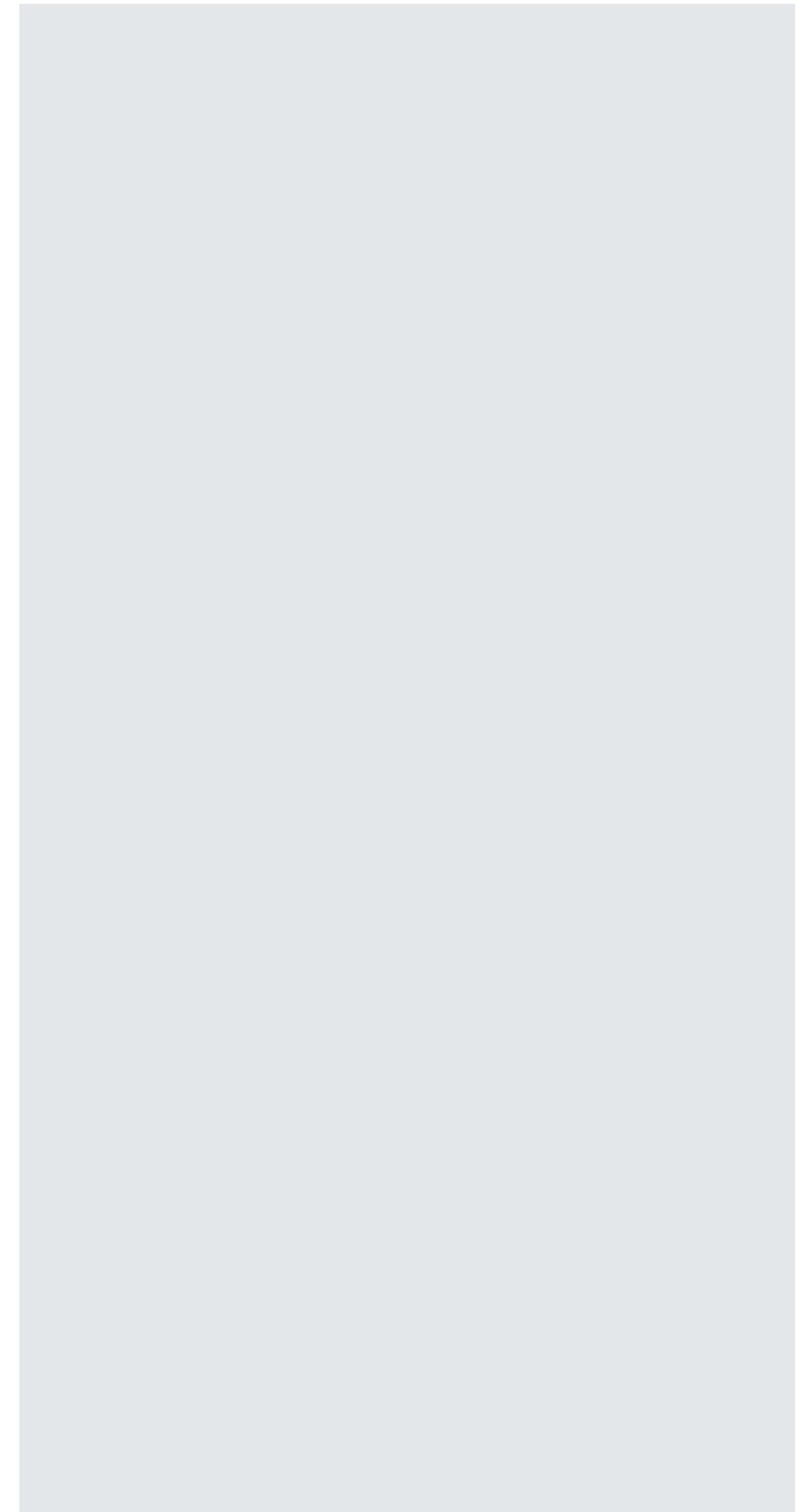
Existing improvements in the Harbor's four developed parks consist primarily of irrigated turf with occasional shade trees, benches, trash receptacles, barbecue grills, picnic tables, scattered planting beds in Harbor View Park, and a planting bed near the sign identifying Channel View Park. Peninsula Park also has two tennis courts and a playground, and Channel View Park has a donated seal sculpture. Public restrooms are available at Harbor View Park, Peninsula Park, Harbor East Park, along the walkway near the Commercial Fishing Marina, at Harbor Landing, near the public boat launch, and near Kiddie Beach. The two-story restroom building at Peninsula Park also has showers for boaters using the public dock nearby, plus a meeting room. On weekends, the parks are well-used for picnics and informal recreation, and annual events throughout the year can draw large crowds. These events include car shows, art fairs, and outdoor concerts. In addition, a weekly farmer's market occurs in the parking lot near Marine Emporium Landing, which is close to Harbor View Park. The parks are also used by dog walkers. Most of the parks are near residential areas—as a result, potential park uses must be considered in light of their impacts to nearby homeowners.



Peninsula Park (parking lot)



Peninsula Park (playground)



existing conditions: signage

Existing Signage

The Harbor currently has a variety of signs throughout most of the site perimeter. The locations of the signs highlight areas for parking and activities.

The two major signs are located at Victoria Ave. and Channels Islands Blvd. (Fig. 8-1) and Channels Islands Blvd. and Harbor Blvd. While the scale of the sign is appropriate in relation to the size of the intersection, the large number of messages displayed on the sign hinders its effectiveness. Considering the speed at which most people drive and how much information they can absorb while driving, there should be no more than three messages displayed per sign. The idea should be that visitors will encounter more signs as they approach specific destination points.

Secondary signs highlighting entry points to parking and businesses are located on the west side of the Harbor (Figs. 8-2 through 8-4). There is inconsistency in the use of the font and placement of the sailboat graphic. Some of these signs also display too many messages. A more effective way to communicate the available services and activities is to place a directory at major pedestrian entry points. A map on the directory, in addition to the list of businesses, is a great benefit to help visitors orient themselves within the site (Fig. 8-5).

Identity

One of the main comments received during the August 22, 2007 public workshop regarding the existing signage is that it does not reflect the Harbor's identity. The sailboat graphic speaks only about one of many activities available in the Harbor. Boaters are certainly a large demographic,

but the Harbor can also appeal to a broader spectrum of users. There is a consensus to create timeless signs that would reflect the unpretentious nature of the Harbor while featuring the area's rich Chumash history and unique Channel Islands wildlife and landscape.

The "face" of the Harbor is on Channel Islands Blvd. This is where most people encounter the Harbor and it is also one of the few areas where one can look down into the water begin to understand the Harbor's size and presence (Fig. 8-6). This view into the Harbor is intriguing and beautiful but more is needed to create interest and motivate someone to park and explore.

Memorable gateway signs placed at Victoria Ave. and Harbor Blvd. would help solidify the presence of the Harbor in relation to its surroundings. Smaller related signs along Victoria Ave. and Harbor Blvd. would perpetuate the Harbor's presence as one moves away from Channel Islands Blvd. and loses site of the water.

When driving from the east side of the Harbor to the west side on Channel Islands Blvd., a person who is unfamiliar with the area can easily miss the left turn onto Harbor Blvd. There is a large triangular median at this intersection (Fig. 8-7), offering an opportunity for a gateway sign and banners. The placement of the sign on the median would make it visible from Harbor Blvd. and Channel Islands Blvd.

The Water Taxi is a great way to facilitate circulation in the Harbor and create a memorable water experience. It is an attractive and useful amenity but currently is not adequately featured (Fig. 8-8). Enhanced pick-up and drop-off zones (Fig. 8-9), along with boat graphics, would create more visibility for the Water Taxi and make it a point of interest in the Harbor.



8-1-Signage at Victoria Ave and CIH Blvd.



8-3-Signage on West Side of Harbor



8-4-Signage on West Side of Harbor



8-2-Signage on West Side of Harbor



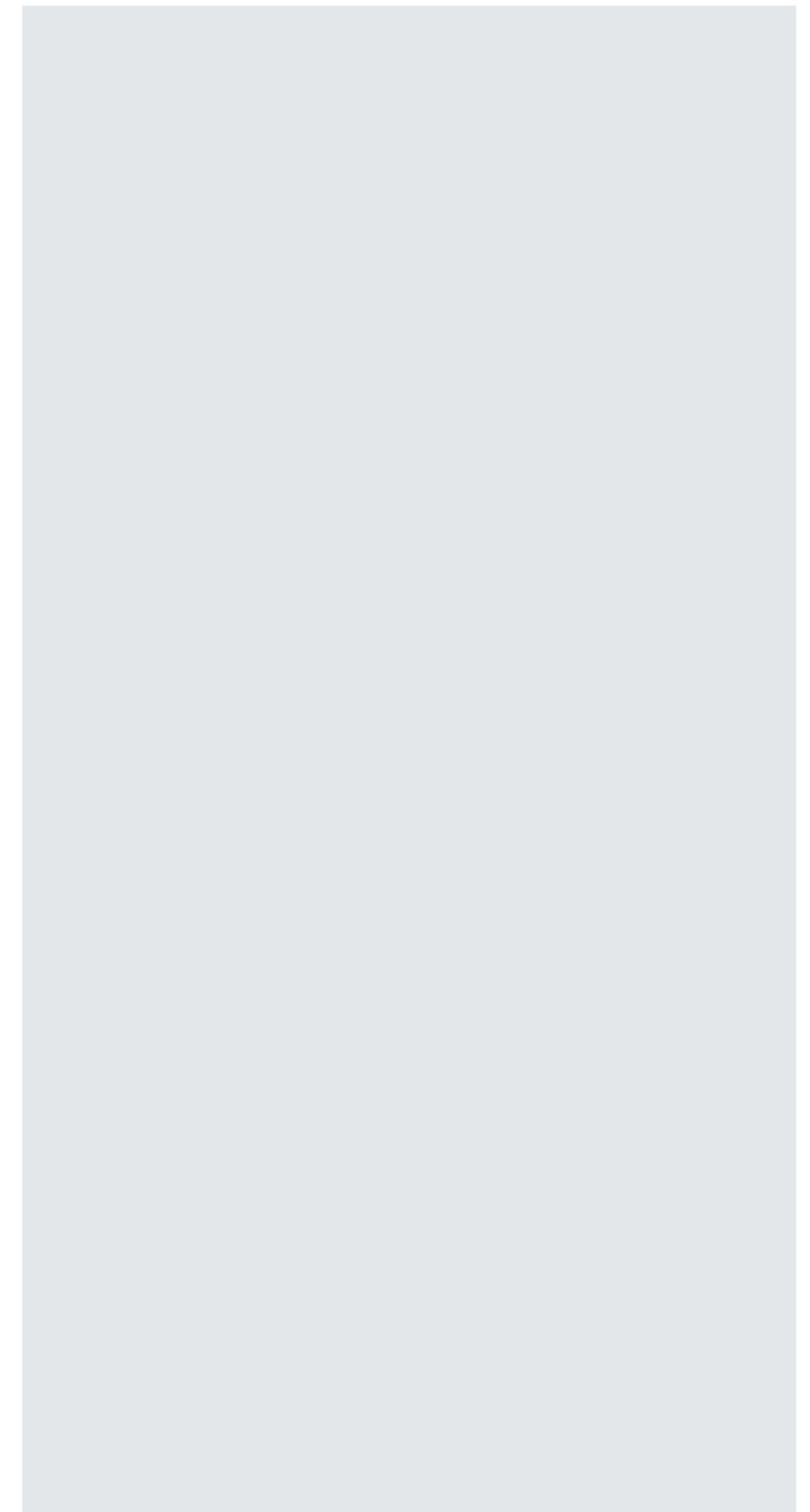
8-5-Example of Directory Signage



8-6-View into the Harbor along CIH Blvd.



8-7- Intersection of CIH Blvd. and Harbor Blvd., good location for new Gateway Signage.



existing conditions: signage

As mentioned above, there is a desire to integrate the Harbor's context and history into the site. There are many opportunities to do so through color, pattern, pavement textures and medallions (Fig. 8-10), banners, and educational panels (Fig. 8-11). The proposed plan for the Harbor includes a continuous pedestrian promenade, an ideal setting for educating visitors as they explore the Harbor. Another way to introduce history and context is through sculptures. They can be integrated into parks and waterfeatures and even encourage interaction and play (Figs. 8-12 and 8-13).

There are several public access points to the two beaches neighboring the Harbor (Fig. 8-14). However, there are no signs directing to the beach or to beach-specific parking. Placing signs at beach access points would help direct people and also create an opportunity to distinguish public vs. private areas given the proximity of private residences.

Parking:

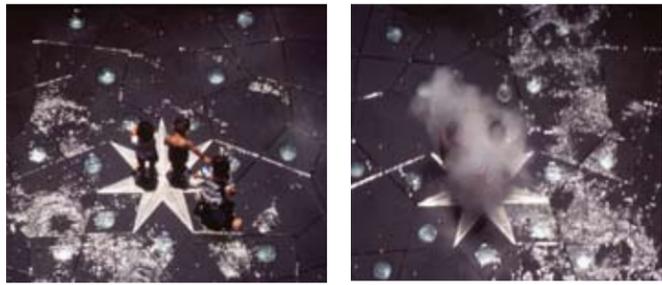
There is a lot of uninviting asphalt around the Harbor. These areas tend to look desolate, especially when they are not being used at full capacity. Banners and wind-activated whirly-gigs (Figs. 8-15 and 8-16) can activate these areas introducing color, animation, and announcements to the public and increase the Harbor's presence in areas where the water is not visible or accessible.

Some of the Harbor's parking areas have specific assigned functions (Fig. 8-17) while others serve as general parking. There are also areas where designated parking for different uses co-exists within a parking lot (Fig. 8-18). As one circulates around the Harbor, it is unclear where these delineations occur and where the public can access water. Parking signs

should be placed at main parking entry points indicating the parking lot function (e.g. public launching ramp, public parking, etc.). The intent is to quickly and easily get visitors to a parking area so they can begin their experience in the Harbor.

Tenant Signage:

There are over 30 different leases in the Harbor, each with a unique situation in relation to the Harbor and surrounding areas. This has led to an assortment of signs all around the Harbor, with different size, shape and visual identity (Figs. 8-18 and 8-19). Many of the residents who frequent the Harbor have commented about the "clutter" of signs and the lack of cohesion across the site (Figs. 8-20 and 8-21). One way to unify the Harbor experience is to develop a set of tenant guidelines outlining sign shapes, sizes, placement, lighting and fence graphics to cover unsightly areas. These guidelines should be flexible enough to accommodate different types of tenant identities while creating a varied but unified sign system throughout the Harbor.



8-12 & 8-13 Example of water features



8-16- Example of wind-activated whirly-gigs



8-17-'Specific function' area along the Harbor



8-14- Existing Beach Access



8-18-Existing assortment of signage around the Harbor



8-19-Existing assortment of signage around the Harbor



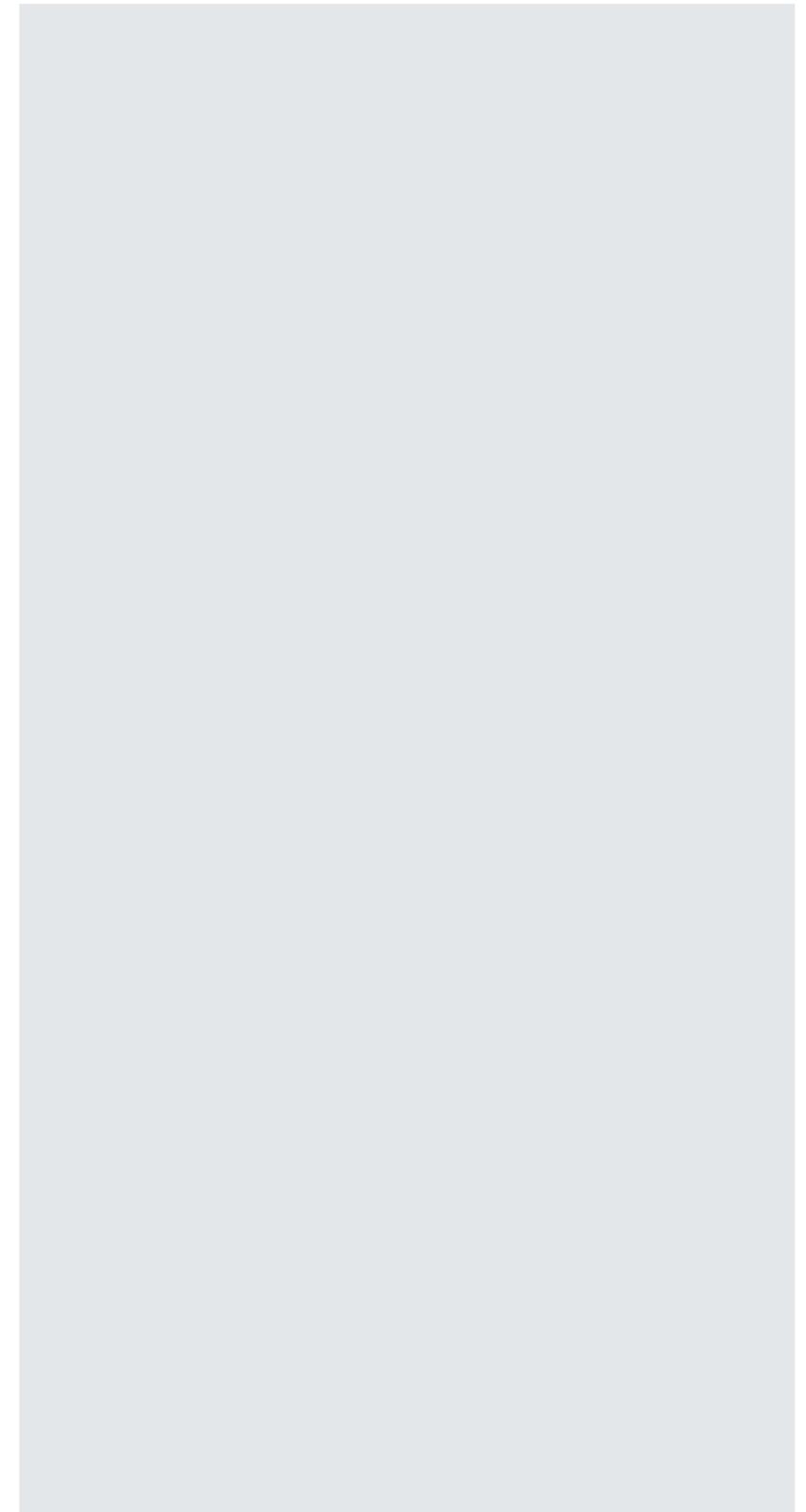
8-15-Banner Example



8-20-Existing Sign Clutter around the Harbor



8-21-Existing Sign Clutter around the Harbor



references

How we got here: Framework

Image credit:

- p. 12 (tomol crossing): Channel Islands National Marine Sanctuary (http://sanctuaries.noaa.gov/pgallery/pgchannel/human/chumash_300.jpg)

Public Walkways

- Bicycle and Pedestrian Facilities Master Plan, City of Oxnard CA (2002).

Parks

- Channel Islands National Park website: <http://www.nps.gov/chis/>
- *Channel Islands National Park* (2000). Susan Lamb. Tucson, AZ: Western National Parks Association.
- *California's Chumash Indians: A Project of the Santa Barbara Museum of Natural History Education Center* (2002). Rosalind Perry, Lynn Roche, Pam Hoeft, Jan Timbrook, John Johnson, Patricia Campbell, and Nick Miller. San Luis Obispo, CA: EZ Nature Books.

Image credits:

- p. 28 (Giant Coreopsis): Derek Lohuis, National Park Service
 - p. 32 (tomol): <http://channelislands.noaa.gov/cr/images/t2.jpg>
 - p. 32 (Arctostaphylos catalinae): David Graber
 - p. 38 (kelp): Kathy DeWet-Oleson
 - p. 46 (grass): Greenlee Nursery
 - p. 46 (shipwreck): Don Morris
-

Landscape

Information sources:

- Bay Area Stormwater Management Agencies Association: Plant Species for Infiltration Areas.
- Brooklyn Botanic Garden: Rain Garden Plants (for California and the Desert Southwest).
- *California Native Plants for the Garden* (2005). Carol Bornstein, David Fross, and Bart O'Brien. Los Olivos CA: Cachuma Press.
- *A Checklist of Vascular Plants of Channel Islands National Park* (1997). Steve Junak, Ralph Philbrick, Sarah Chaney, and Ronilee Clark. Tucson, AZ: Southwest Parks and Monuments Association.
- *Plants of El Camino Real: California Native Plants* (2004). Tree of Life Nursery, San Juan Capistrano, CA.

Image credits:

- p. 58 Calycanthus occidentalis: <http://www.laspilitas.com/plants/127.htm>
 Ceanothus impressus: <http://www.laspilitas.com/plants/154.htm>
 Prunus ilicifolia ssp. ilicifolia: BonTerra Consulting
- p. 60 Ceanothus 'Wheeler Canyon': <http://www.laspilitas.com/plants/1093.htm>
 Eriogonum giganteum: <http://www.timetotrack.com/jay/stcath.jpg>
 Ribes viburnifolium: http://www.wildscaping.com/plants/plantphotos/Rib_viburn_400dm.jpg
 Salvia leucophylla 'Point Sal': <http://www.californiagardens.com/images/Salvia-leucophylla-pt-sal-3.jpg>
- p. 62 Artemisia nesiotica: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=0000+0000+0606+0201
 Artemisia palmeri: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=0000+0000+1100+0019
 Eriogonum parvifolium: http://www.yerbabuenanursery.com/images/new_botimages/large/0242_s.jpg
 Eriophyllum nevinii: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=0000+0000+0502+0791
 Galvezia speciosa: http://www.yerbabuenanursery.com/images/new_botimages/large/0263_2_s.jpg
 Salvia brandegei: <http://www.laspilitas.com/plants/pictures/a605.jpg>

references

Landscape image credits (continued from p. 145):

- p. 62 *Venegasia carpesioides*: <http://www.calflora.net/bloomingplants/images/canyonsunflower10.jpg>
- p. 64 *Arctostaphylos uva-ursi*: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=8120+3181+4014+0134
Artemisia californica 'Canyon Gray': <http://www.laspilitas.com/plants/pictures/a94.jpg>
Baccharis pilularis 'Pigeon Point': http://www.agri.gov.il/Publications/OpenDoor/Sima/pics/Baccharis_pilularis_Pigeon.jpg
Ceanothus maritimus: <http://www.laspilitas.com/plants/161.htm>
- p. 66 *Artemisia douglasiana*: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=5161+1631+2435+0034
Calystegia macrostegia: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=0000+0000+0502+0786
Camissonia cheiranthifolia: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=0000+0000+1202+0107
Castilleja affinis: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=5207+1611+1109+0096
Coreopsis maritima: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=0000+0000+0606+0398
Dudleya virens: http://calphotos.berkeley.edu/cgi/img_query?query_src=photos_flora_sci&enlarge=0000+0000+0603+0642
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Signage

Image credits:

- p. 79 Top right – sailing crew: Shannon Green for Corbis
Bottom right – mast: Jason Edwards
- p. 80 Top middle – humpback whale: Wayne Levin
Top right – killer whale: Amos Nachoum for Corbis
Bottom left – pelican: Arthur Morris for Corbis
Bottom middle – heron: Arthur Morris for Corbis
Bottom right – dolphin: Stuart Westmoriand
- p. 81 Top right – star fish: Raymond K. Gehman for National Geographic
Bottom left – gopher rockfish: Brandon Cole
- p. 82 Compass: Greg Pease
Rope: Hans Neleman

Drainage & Water Quality and Pavement, Parking & Vehicular Circulation

- Clean Marinas California Program
- City of Oxnard Standard Plans
- California Stormwater Quality Association Stormwater Best Management Practice (BMP) Handbooks
- California Regional Water Quality Control Board Los Angeles Region Waste Discharge Requirements from Storm Water (Wet Weather) and Non-Storm Water (Dry Weather) Discharges from the Municipal Separate Storm Sewer Systems within the Ventura County Watershed Protection District, County of Ventura and the Incorporated Cities Therein (“MS4”)
- Channel Islands Harbor Westside Parking Demand Study (pending)
- Maintenance and Repair of Asphalt Parking Lots (Asphalt Pavement Institute)

record of final public hearing & adoption

BOARD MINUTES
BOARD OF SUPERVISORS, COUNTY OF VENTURA, STATE OF CALIFORNIA

**SUPERVISORS STEVE BENNETT, LINDA PARKS,
KATHY I. LONG, PETER C. FOY AND JOHN K. FLYNN**
June 24, 2008 at 1:30 p.m.

404.2

Public Hearing and Adoption of Channel Islands Harbor Public Areas Plan and Design Guidelines. (Harbor Department)

- (X) All board members are present.
- (X) The following persons are heard: Lyn Krieger, Marilyn Miller, Bob Nahm, and Penny Boehm.
- (X) The following document is submitted to the Board for consideration:
(X) PowerPoint Presentation: Channel Islands Harbor Public Areas Plan and Design Guidelines.
- (X) The Board holds a public hearing.
- (X) Upon motion of Supervisor Flynn, seconded by Supervisor Long, and duly carried, the Board hereby approves the attached staff recommendations.

By: 
Rosa Gonzalez
Deputy Clerk of the Board

CLERK'S CERTIFICATE
I hereby certify that the annexed instrument is a true and correct copy of the document which is on file in this office.
MARTY ROBINSON, Clerk of the Board of Supervisors, County of Ventura, State of California.

Dated: _____

By: _____
Deputy Clerk of the Board

Item #45
6/24/08

DISTRIBUTION: Originating Agency, Auditor, File



Lyn Krieger
Director

CHANNEL ISLANDS HARBOR
Ventura County Harbor Department
3900 Pelican Way • Oxnard, CA 93035-4367



Telephone (805) 382-3001
FAX (805) 382-3015
www.channelislandsharbor.org

June 24, 2008

Board of Supervisors
County of Ventura
800 S. Victoria Ave.
Ventura CA 93009

SUBJECT: Public Hearing and Adoption of Channel Islands Harbor Public Areas Plan and Design Guidelines

Recommendation:

Harbor Department staff recommends that your Board:

1. Conduct a public hearing on the Draft Public Areas Plan and Design Guidelines and Opportunities and Constraints Report ("Guidelines");
2. Find, based on the substantial evidence contained in this letter and any oral presentation made to your Board on this matter, that the work described herein related to the Channel Islands Harbor Public Areas Plan and Design Guidelines, is exempt from the terms of the California Environmental Quality Act (CEQA) for the reasons set forth herein; and
3. Adopt the Resolution approving the Guidelines for application to future development in the Harbor.

Fiscal/Mandates Impact:

Mandatory	No
Source of Funding	N/A
Funding Match Required	No
Impact on Other Departments	None

Summary of Revenue and Costs:

Other than staff and previously approved consultant time, there are no costs associated with this study session.

Board of Supervisors
June 24, 2008
Page 2

Discussion:

On June 26, 2007, your Board approved a contract with Wallace Roberts and Todd, Inc. (WRT), to prepare a plan for the parks and public amenities in Channel Islands Harbor. At the inception of the contract, WRT reviewed all Harbor spaces and equipment, such as parking lot lighting, and completed an "Opportunities and Constraints" report. The results of this work are contained in a document entitled "Channel Islands Harbor Opportunities and Constraints Report Landside Enhancements." This report is on line at the Harbor Department web site (www.channelislandsharbor.org).

Also as part of the contract services, WRT conducted two public workshops, one meeting with a group of Harbor users who frequently walk in the Harbor, and two workshops with the Harbor Commission. There were two study sessions with your Board on October 2, 2007, and February 12, 2008. The results of all of the work by WRT is contained in the document entitled "Channel Islands Harbor Public Areas Plan and Design Guidelines" (Guidelines). This report is also available on line at the Harbor Department web site.

The Guidelines were released for public review on May 10, 2008. Notices of the availability of the draft were inserted in all the utility bills sent to Channel Islands Beach Community Services District customers (approximately 1,800), sent to all the attendees at the public meetings who left their addresses on the sign in sheets, and sent to the Harbor's list of people and agencies interested in the Harbor. In total, approximately 2,500 notices were mailed. The two documents have been available on the Harbor's website since May 10th, available for free to the public in CD format, or available for purchase in paper form.

The framework of the Guidelines incorporates the Channel Islands to theme each of the five parks within the Harbor. Each park would be redesigned to represent an island using landforms, landscaping, history, and cultural elements. For example, the long, linear park along Harbor Blvd. would be representative of Santa Cruz Island. Santa Cruz is the largest island in the Channel Islands chain, has the greatest diversity of plants, has the world's largest sea cave, and had a large population of Chumash. The park would include a demonstration garden with coastal dune plants, an interpretative plaza, signature trees, seat walls and benches, a children's play area with equipment designed around Chumash or native landform themes, and the addition of picnic tables and benches. Existing trees would be retained.

A park addition and some park expansions are also proposed. An additional park is proposed just south of the Channel Islands Boulevard bridge, adjacent to the City of Oxnard Fire Station. This currently bare area is used by residents of the Harbor and Mandalay Bay as a walk through.

Board of Supervisors
June 24, 2008
Page 3

The Plan also includes guidelines for the public sidewalk and pedestrian amenities, lighting, landscaping, signage, parking, and drainage and water quality.

Construction of the improvements will be carried out with the development of each leasehold parcel. Individual leases require the lessees to implement the County's approved guidelines for the installation of public improvements. For example, with the remodel of Channel Islands Landing and Boatyard, the lessee will install two public walkways. The first is a new public walkway with landscaping and lighting along and near the water. The second will provide curb, gutter and sidewalk with street trees and a landscaping buffer along Victoria Avenue along the entire length of the parcel. Proposed redevelopment plans for Channel Islands Landing, Anacapa Marine Services, Fisherman's Wharf, Marine Emporium Landing, the Casa Sirena, and the public launch ramp will complete a majority of waterfront promenade and walkway spaces over the next five years. Remaining, uncompleted parcels as to promenade will include the apartments on the peninsula and the far southwest corner of the Harbor. These will lag until redevelopment occurs. It will be up to the County to make improvements along publicly held parcels, such as the County maintenance yard.

With Board approval, park improvements will be funded through the Harbor Enterprise Fund, with assistance from the City of Oxnard, and from grants that may be available. The memorial bench program will also be wrapped into the improvement plan. The Harbor Department will be proposing a pilot project to develop one or two of the proposed parks in the initial phase.

Your Board's approval of the Public Areas Plan and Design Guidelines is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) because implementation of the design guidelines constitutes repair, maintenance or minor alterations of existing facilities; replacement or reconstruction of existing structures; construction of limited new small facilities; minor alterations to land; and actions to maintain, protect, or restore natural resources (CEQA Guidelines Sections 15301, 15302, 15303, 15304, 15307 and 15308).

The County Executive Office, County Counsel, and Auditor Controller's Office have reviewed this letter. If you have any questions regarding this matter, please contact Marilyn Miller at 382-3005.


LYN KRIEGER
Director

record of final public hearing & adoption

**RESOLUTION OF THE BOARD OF SUPERVISORS
OF THE COUNTY OF VENTURA
ADOPTING THE CHANNEL ISLANDS HARBOR
PUBLIC AREAS PLAN AND DESIGN GUIDELINES**

WHEREAS, the County of Ventura contracted with Wallace Roberts & Todd, Inc., to prepare a Public Areas Plan and Design Guidelines for use in redevelopment and reconstruction of Channel Islands Harbor; and

WHEREAS, the Harbor Department, with Wallace Roberts & Todd, Inc., held two public workshops, two Harbor Commission workshops, and two workshops at the Board of Supervisors to obtain the public's input on the public amenities in the Harbor; and

WHEREAS, notices of these public hearings were sent to interested individual and agencies, were included in the utility bills mailed to customers of the Channel Islands Beach Services District, mailed to City of Oxnard neighborhood council chairs, and mailed to slip tenants within the Harbor, residents of the Seabridge, Mandalay Bay, and Westport communities in the City of Oxnard; and

WHEREAS, Wallace Roberts & Todd, Inc., documented the findings from these public workshops, as well as their assessment of the existing conditions in Channel Islands Harbor in a report entitled "Channel Islands Harbor Opportunities and Constraints Report Landside Enhancements," dated April 24, 2008; and

WHEREAS, the DRAFT Channel Islands Harbor Public Areas Plan and Design Guidelines and the Opportunities and Constraints Report were released for public review on May 10, 2008, and have been available in paper format from the Harbor offices, on CD from the Harbor offices, and on the Harbor's website since that date; and

WHEREAS, the Channel Islands Harbor Public Areas Plan and Design Guidelines is intended to assist in the redevelopment of the Harbor parcels by encouraging cohesive public amenities such as parks, pedestrian promenades, landscaping, lighting, signage and parking areas; and

WHEREAS, the Channel Islands Harbor lessees have agreed to be bound by these Plans and Guidelines when designing their development projects; and

WHEREAS, the Channel Islands Harbor Public Areas Plan and Design Guidelines will be used to prepare the Landside Public Works Plan Amendment for the Channel Islands Harbor (PWPA #4), and in preparing any Notice of Impending Development documents for future development in the Harbor; and

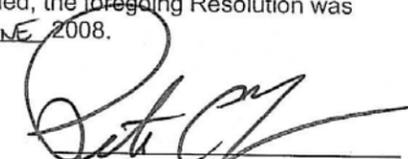
WHEREAS, the Channel Islands Harbor Public Areas Plan and Design Guidelines will assist the County in obtaining any grant funds or other assistance from other public agencies when developing park areas, or constructing other public facilities; and

WHEREAS, the Plan anticipates replacement and reconstruction of existing facilities, the addition of small park areas, signage and lighting, and improvements to parking areas and landscaping that are designed to improve the quality of storm water runoff into the Harbor and reduce the use of water for landscaping purposes.

NOW, THEREFORE, BE IT RESOLVED, ORDERED, AND DETERMINED THAT the Board determines that the approval of the Channel Islands Harbor Public Areas Plan and Design Guidelines is Categorically Exempt from the California Environmental Quality Act (CEQA) under Sections 15301, Existing Facilities, 15302, Replacement or Reconstruction, 15303, New Construction or Conversion of Small Structures, 15304, Minor Alterations to Land, and 15307 and 15308, Actions to Maintain, Protect, or Restore Natural Resources.

FINALLY, BE IT RESOLVED, ORDERED, AND DETERMINED THAT the Channel Islands Harbor Opportunities and Constraints Report and Channel Islands Harbor Public Areas Plan and Design Guidelines are hereby adopted.

Upon motion of Supervisor FIVNN, seconded by Supervisor LONG, duly carried, the foregoing Resolution was passed and adopted this 24th day of JUNE 2008.


Chair, Board of Supervisors

ATTEST: Marty Robinson,
Clerk of the Board of Supervisors,
County of Ventura, State of California.



By: 
Clerk of the Board